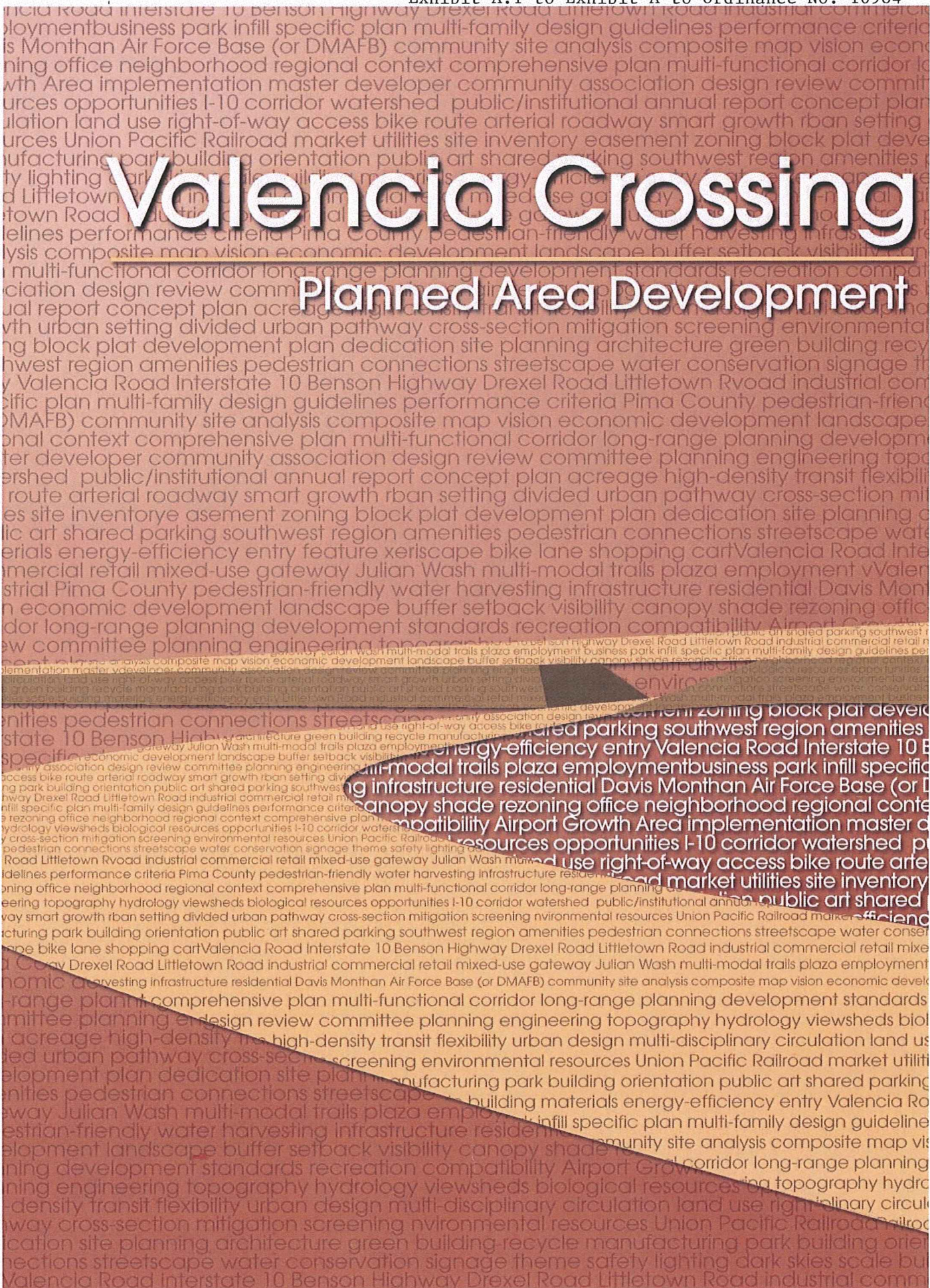


Valencia Crossing

Planned Area Development



Valencia Crossing Planned Area Development
Valencia Road and Interstate 10
Tucson, Arizona

Submitted to:

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Valencia Crossing

Section I - Introduction

A. Project Overview

The Valencia Crossing Planned Area Development (PAD) encompasses approximately 195 acres located in an infill area within the City of Tucson (See Exhibit I: Location Map, page 3.) The subject property is situated to the north and south of Valencia Road and bisected by Interstate 10. The project site was annexed into the City of Tucson in 2011 and was given translational zoning categories of R-1, C-2 and I-1. A change in zoning to Planned Area Development (PAD) is requested for development of the site.

The opportunities and constraints identified in the Site Inventory were taken into consideration in the creation of the Valencia Crossing Development Plan, including the Land Use, Circulation and Post-Development Hydrology Concepts. The Valencia Crossing Planned Area Development establishes development standards and design guidelines for the implementation of the Development Plan and the overall project vision.

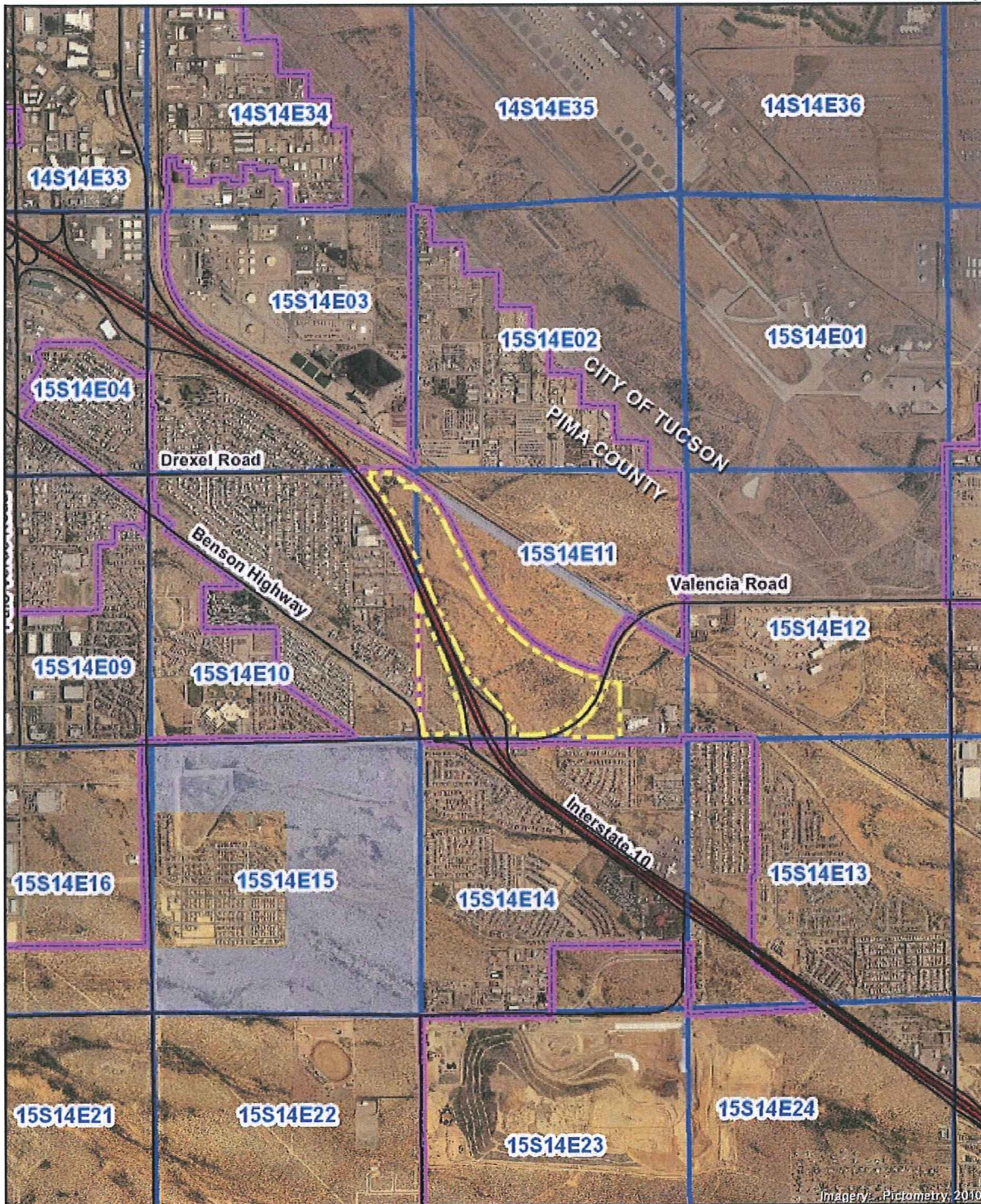
Surrounded by existing infrastructure, this infill development project is envisioned to exhibit a mix of uses, although the majority of the PAD area will consist of employment generating land uses, including commercial and business park uses. Approximately 85 acres of land closest to Valencia Road and Littletown Road have been designated for commercial/retail. Commercial at this location is key to support nearby residents and passersby traveling along Interstate 10 and Valencia Road. Approximately 73 acres is designated for mixed-use development, north of the commercial area, which could result in commercial, office, industrial, public/institutional or residential land uses (or a combination thereof). Additionally, approximately 30 acres west of Interstate 10 is designated for industrial uses and 7 acres at the northern property line is designated for drainage/recreation.

B. Rationale and Benefits for Use of a PAD






The City of Tucson Planned Area Development (PAD) zoning designation is intended to provide land use guidance and design standards for the future development of Valencia Crossing. Prior to development of any area within Valencia Crossing, detailed development and design standards will be submitted as an extension of this document. It will allow for the entire 195 acres to be designed as a master planned community rather than developing in a piecemeal fashion. Valencia Crossing will accommodate growth by providing for a mixture of uses, including employment-generating uses, such as office, commercial and retail centers, and industrial uses as well as higher residential densities.



Exhibit I: Location Map



LEGEND

-  Site Boundary
-  Davis Monthan Air Force Base
-  Interstate
-  Jurisdiction Boundary
-  State Trust Land



FILE NAME: FTP-02_regionalLocation.mxd
 SOURCE: Pima County DOT GIS, 2010



C. Conformance with the General Plan

Valencia Crossing PAD conforms to the long range vision of the City of Tucson General Plan by guiding development to take advantage of the existing infrastructure and multi-modal opportunities in an urban infill area. The site is bisected by Interstate 10 within the Evolving Edge Growth Area of the City of Tucson. This area has recently experienced an increase in single-family residential development, and as a result, there is a lack of nearby commercial services. Recommendations for land use in this area include Commercial and Industrial uses along the Interstate 10 Corridor.

1. Evolving Edge Growth Area

The Evolving Edge Growth area promotes policies, programs and improvements which support a compact, transit-oriented jobs-housing balance within the I-10 Corridor. The goal of the PAD is to support infill development along the Interstate 10 corridor for high-density residential, commercial and employment needs in the area surrounded by single-family residential, industrial uses and Davis Monthan Air Force Base.

2. Access and Circulation

In accordance with the Livable Tucson Goals, the alternatives to automobile transportation include improvement of public transportation, improvement of roadways (lighting, landscaping, sidewalks and bus stops) and promotion of alternatives to the automobile. The PAD will improve:

- Accessibility to the existing bus stops on Valencia Road
- Bus Service connecting from Valencia Road to Drexel Road.
- Proximity to the Union Pacific Railroad line.
- Landscaping (including passive water harvesting), lighting and pedestrian accessibility.
- Routes for walking and biking from adjacent residential areas to commercial services (includes the proposed Julian Wash Greenway).

3. Community Character and Design

The Community Character and Design element supports infill and redevelopment projects that reflect sensitivity to the site and neighborhood conditions, and adhere to site and architectural design guidelines.

The PAD supports the policies of the Community Character and Design element of the General Plan by providing design guidelines to ensure development is compatible with the existing neighborhood character especially where adjacent to single family residential uses.

4. Economic Development

The Economic Development element encourages efforts to recruit new businesses and industries to Tucson. The goal of the Valencia Crossing PAD is to attract commercial and business park uses to an infill area that features multi-modal transportation opportunities and affordable housing close to work.



5. Parks, Recreation, Open Space and Trails

The regional vision for parks, open space, trails, and recreational opportunities responds to the beauty and unique ecology of the Sonoran Desert. This area has a long history of open lands and active, outdoor, year round recreation. One of the goals of the Valencia PAD is to assist the build-out and continuation of the Julian Wash Greenway. This project, a multi-use trail envisioned as a segment of a more than 12-mile trail system linking southeast Tucson with the downtown area, the Julian Wash Linear Park Master Plan provides for linkages to future trail extensions and existing trail systems, adjacent neighborhoods, and the University of Arizona Science and Technology Park.

D. Rincon/Southeast Subregional Plan

The site is located within the Rincon/Southeast Subregional Plan (RSSP). The RSSP covers approximately 400 square miles, the majority of which is rural and sparsely populated. There are no specific planned land use policies for the subject property; however, industrial and commercial uses respond to existing development patterns within or adjacent to the I-10 corridor, including Tucson International Airport and Davis-Monthan Air Force Base, the Tucson Electric Power generating plant, a major landfill, and Federal and State Prisons.

E. Compatibility with Adjoining Land Uses

The majority of the land uses surrounding the project site are currently industrial, single-family residential, commercial and vacant. Davis Monthan Air Force Base (DMAFB) is located approximately one mile north and east of the site. The proposed land use concept takes into consideration DMAFB and the Airport Environs Height Overlay Zone.

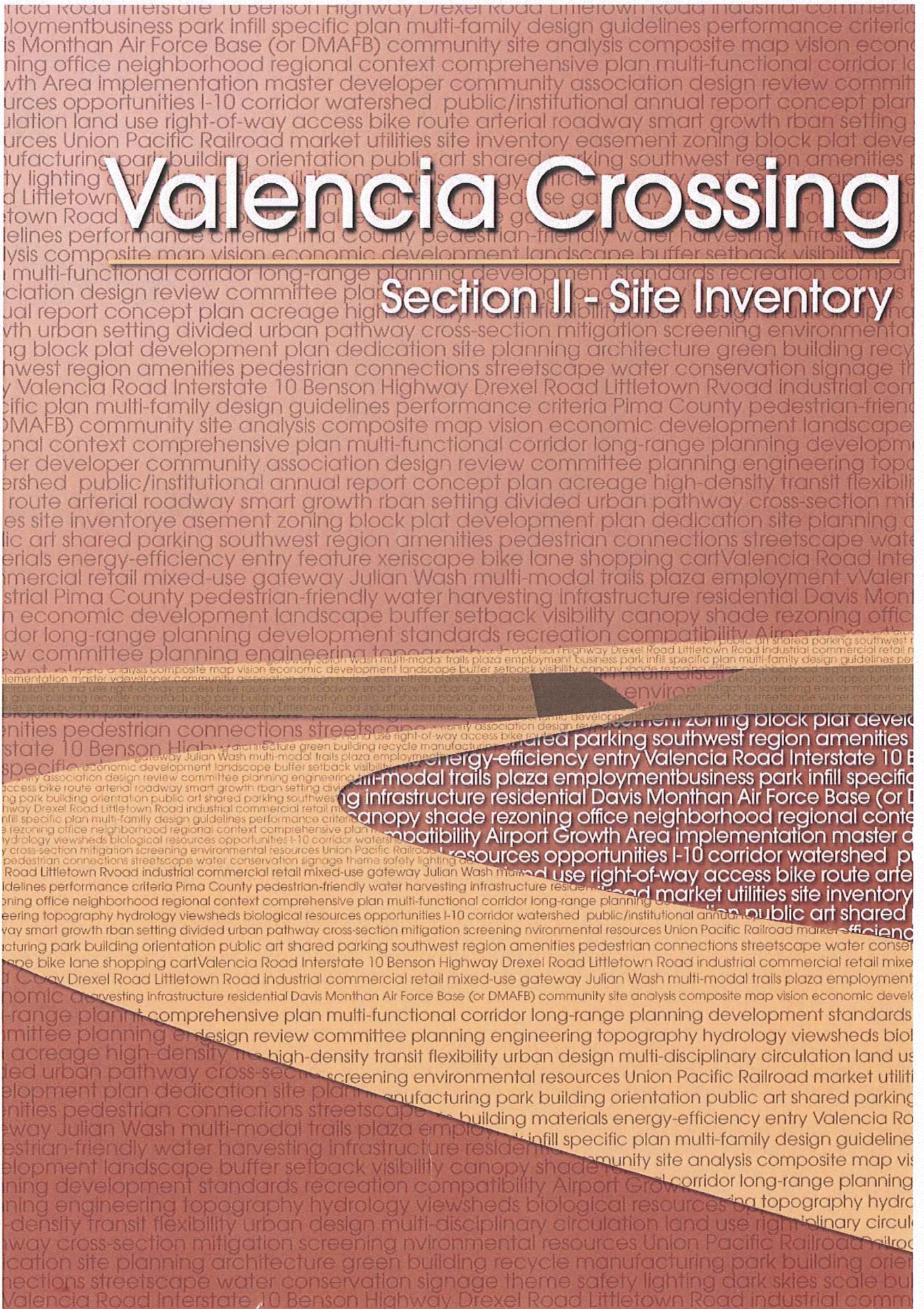
The Valencia Crossing PAD is designed to be compatible with the surrounding developments and complement the mixed-use character of the area with additional regional commercial, employment centers and high-density residential along the I-10 and Valencia corridor. The immediate area surrounding the site consists of major rights-of-way including Interstate 10, Valencia Road, Benson Highway and Drexel Road, as well as Union Pacific Railroad running parallel to the site to the east between vacant land and the Julian Wash Corridor located along the eastern boundary of the site. The vacant land to the east (in incorporated Pima County) is currently zoned for CR-3 (Single Family Residential and CI-1 (Light Industrial), which are both compatible with the industrial and high-density residential uses proposed in the PAD. Areas to the north between the site and the Union Pacific Railroad are also vacant.

Land uses to the south across Valencia Road and Littletown Road consist of one and two-story single-family residential subdivisions and a gas station/convenience store built within the last 10 years. These neighborhoods will benefit from the proposed neighborhood commercial uses. Valencia and Littletown Roads provide a setback, along with proposed buffers, and screen walls, as described in Section III.D: Landscape Border Plan. Land uses adjacent to the west and north of the subject property consist of industrial subdivisions and some scattered residences. Appropriate screening and buffering will be applied when adjacent to any residential uses to ensure the proposed uses are compatible with the industrial planned for the parcel west of Interstate 10.



Valencia Crossing

Section II - Site Inventory



A. Existing Land Uses**1. Location and Regional Context**

The project site is located in the southeastern portion of the City of Tucson. The 195-acre PAD area is bisected by Interstate 10 and located south of Drexel Road and on either side of Valencia Road. The site is comprised of 26 parcels within Township 15 South, Range 14 East, Sections 10 and 11. All parcels are under the ownership of Stewart Title and Trust TR 2549. (See *Exhibit I: Location Map*, page 3.)

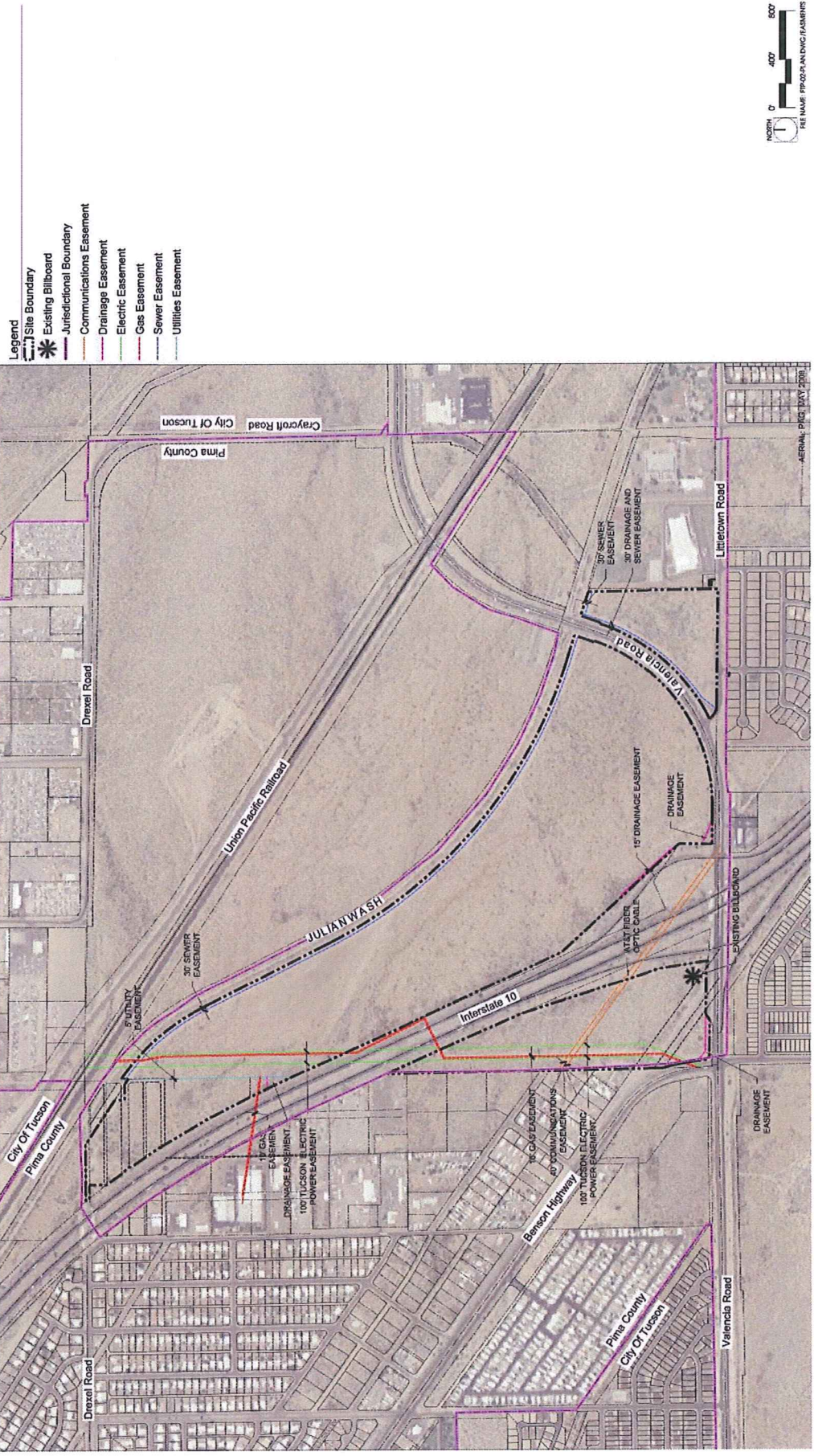
2. Existing On-Site Land Uses

The site is vacant. *Exhibit II.A.2: Existing On-Site Land Uses* on page 8, indicates the locations of various easements and one billboard on the site. The parcels located west of Interstate 10 have Tucson Electric Power transmission lines running from north to south within a 100-foot electric easement. The transmission lines cross Interstate 10 to the northern portion of the parcel east of Interstate 10 to the Tucson Electric Power Plant located north of the site. There is also a 10-foot gas easement located within this 100-foot electric easement. A communications easement and a drainage easement run through the parcel located west of Interstate 10.

The parcels east of Interstate 10 contain a sewer easement alongside the Julian Wash north and south of Valencia Road. The sewer easement also runs along the frontage of the parcel on the south-side of Valencia Road within a 30-foot drainage easement. A 15-foot drainage easement is located along the Interstate 10 ramp at the southwest boundary of the eastern parcel, and a 5-foot utility easement is located on the north end of the eastern parcel.



Exhibit II.A.2: Existing On-Site Land Uses



3. Existing Land Uses within One-Quarter-Mile Radius

a. Existing On-Site and Off-Site Zoning

Table II.A.3.a: Existing Zoning

Project Site	City of Tucson C-2, R-1 and I-1
North	CI-1, CI-2, CR-3 and City of Tucson I-2
South	CR-3, CB-2, TH and CR-4
East	CR-3, CI-1 and City of Tucson R-1
West	CI-1, CB-2, CR-3, CMH-1 and TH

(See Exhibit II.A.3.a: Existing Zoning, page 10.)

b. Existing Off-Site Land Uses

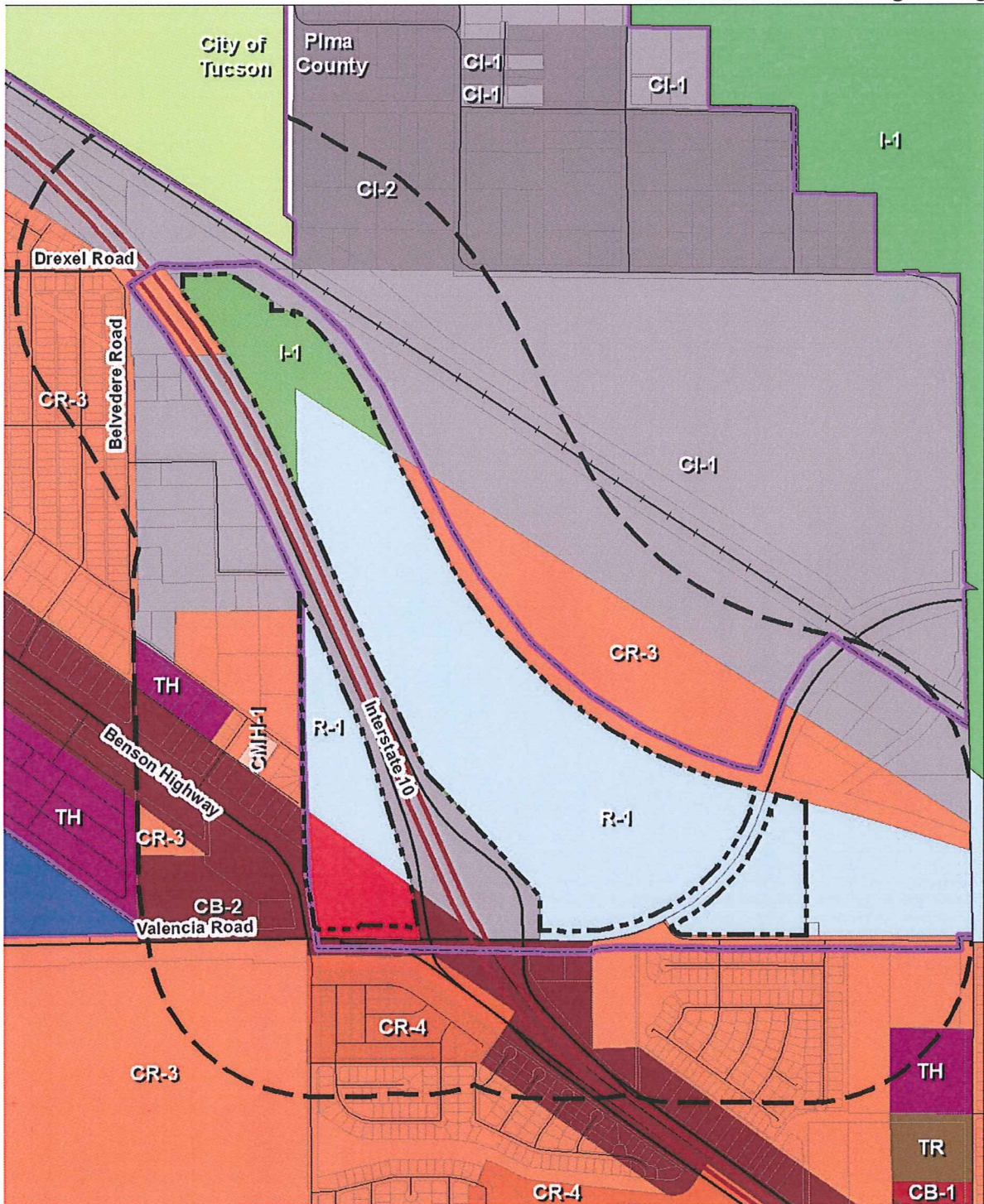
Table II.A.3.b: Existing Off-Site Land Uses

North	Vacant land, Julian Wash, Drexel Road, Union Pacific Railroad, Tucson Electric Power generating station and auto salvage/wrecking yard operation
South	Vacant land, Valencia Road, fast food restaurant, gas station, single-family residential, RV park
East	Julian Wash, vacant land, Union Pacific Railroad (4 tracks) Littletown Road, Craycroft Elementary School and Hanner Middle School
West	Benson Highway, single-family residential, mobile home, multi-family housing, commercial, vacant land, light industrial, warehouse and office

(See Exhibit II.A.3.b: Existing Off-Site Land Uses, page 11.)



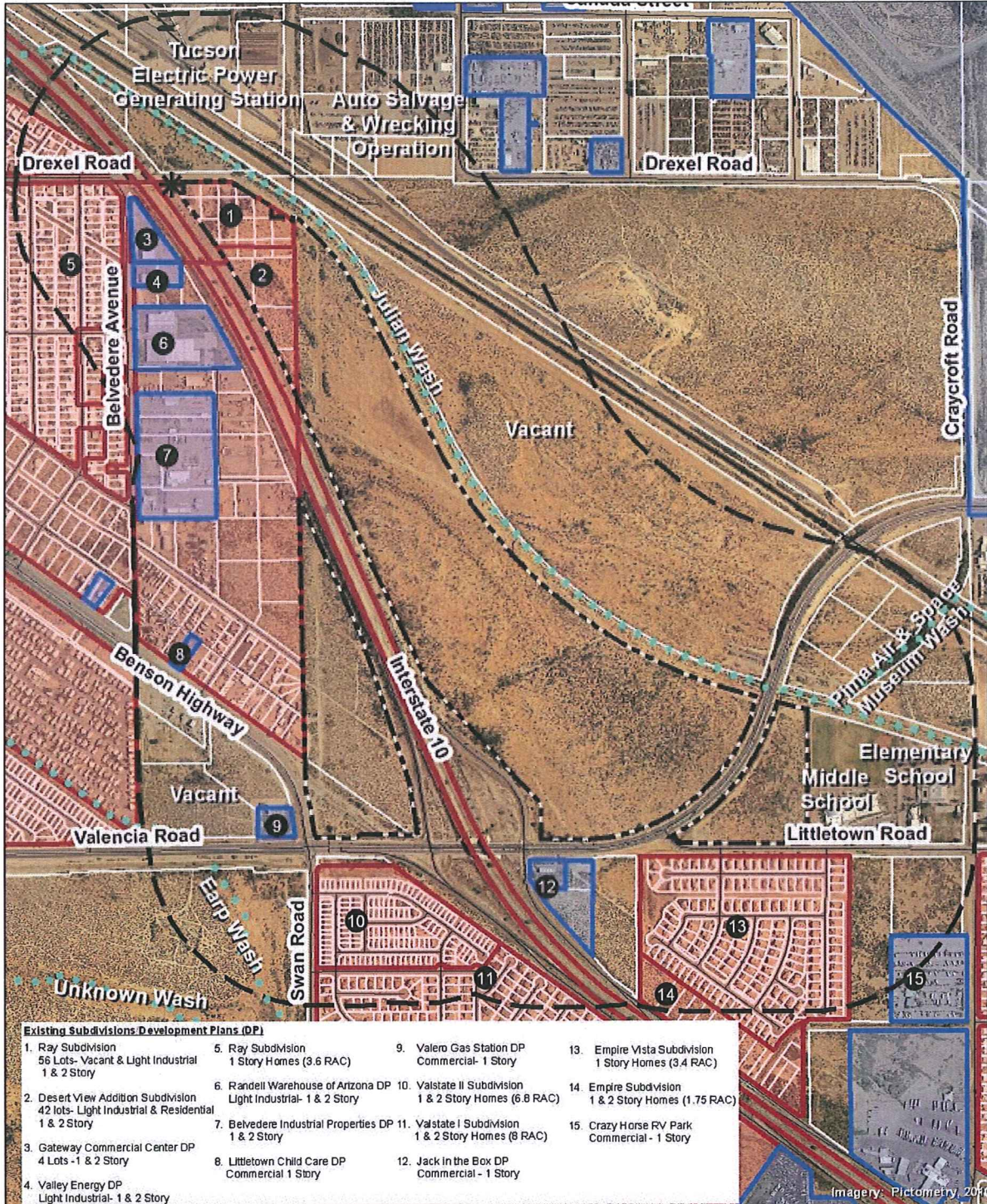
Exhibit II.A.3.a: Existing Zoning



LEGEND		Pima County Zoning		City of Tucson Zoning		 FILE NAME: FTP-02_zoning.mxd SOURCE: Pima County DOT GIS, 2011
	Site Boundary		CB-1		R-1	
	Jurisdictions		CB-2		I-1	
	Quarter Mile Radius		TH		C-2	
	Union Pacific Railroad		CI-1		R-2	
			CI-2		CR-3	
			CR-3		I-2	
			CR-4		R-1	
			CMH-1		R-1	
			TH		R-1	

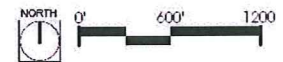


Exhibit II.A.3.b: Off-Site Land Uses



LEGEND

- Site Boundary
- Quarter Mile Radius
- Approved Subdivision Plat
- Approved Development Plan
- Washes
- Union Pacific Railroad
- Existing Billboard



FILE NAME: FTP-02_existinglanduses.mxd
SOURCE: Pima County DOT GIS, 2011



B. Community Facilities

1. Fire and Police Stations

The site is not within a designated fire district. The nearest fire station, Rural Metro Fire Station #71, is located approximately 1500 feet west of the PAD at 4445 East Benson Highway. The Tucson Fire Department District is located just west of the Rural Metro Fire Station. (See *Exhibit II.B: Community Facilities*, page 13.)

There are no police stations within one-mile of the PAD District.

2. Hospitals

There are no hospitals within one-mile of the PAD District.

3. Schools

The site is located within Sunnyside School District (SUSD). Currently, the site will be served by Craycroft Elementary School and Billy Lane Lauffer Middle School, located directly east of the parcel south of Valencia Road, and Desert View Middle School, located approximately 3500 feet west of the site along Valencia Road. The current enrollments for these schools are:

Craycroft Elementary School	710
Billy Lane Lauffer Middle School	580
Desert View High School	2152

4. Parks, Recreation Areas and Public Trails

As indicated in *Exhibit II.B*, page 13, there are two parks within a one-mile radius of the site: Thomas Jay Regional Park and Augie Acuña Los Niños Neighborhood Park. The Thomas Jay Littleton Regional Park is located approximately 0.25 mile to the east of the site. Augie Acuña Los Niños Neighborhood Park is located approximately 0.80 mile to the northwest of the project site. Additionally, the Pima Air and Space Museum is located approximately 0.50 mile to the east of the site.

5. Proposed Trail Rights-of-Way

The Eastern Pima County Trail System Master Plan identifies two trail rights-of-way within a one-mile radius of the project site. The Julian Wash Greenway is located along the northern boundary of the project site along the Julian Wash; however, there are currently no dedicated trail rights-of-way. The planned route begins at Palo Verde Road and ends at Kolb Road. The other trail is shown along the Earp Wash approximately 1500 feet south of the project site; right-of-way also does not currently exist. (See *Exhibit II.B: Community Facilities*, page 13.) The Earp Wash Trail begins at the Rodeo Wash Detention area and ends at Los Reales Road.



Exhibit II.B: Community Facilities



Local Parks/Trail Rights-of-Way/Schools	
1. Aguile Acuna Los Ninos Neighborhood Park	5. Thomas Jay Regional Park
2. Julian Wash Trail	6. Earp Wash Trail
3. Lauffer Middle School	7. Desert View High School
4. Craycroft Elementary School	8. Los Ninos Elementary School

LEGEND

- Site Boundary
- One-Mile Radius
- Interstate
- Park
- Trail Right-of-Ways (Eastern Pima County Trail System Master Plan)
- Fire Stations
- Tucson Fire Department District
- City of Tucson Owned Parcels

NORTH 0' 1,300' 2,600'

FILE NAME: FTF-02_recreation.mxd
SOURCE: Pima County DOT GIS, 2010



C. Traffic

1. Existing Off-Site Streets

The property is currently vacant but is accessible via Drexel Road along the north boundary, Valencia Road along the south, and Benson Highway along the west boundary.

Interstate 10 traverses the City of Tucson in a northwest to southeast direction and is located through the southwest portion of the project site with the Valencia Road entrance ramp near the southern boundary of the site.

Valencia Road is a divided four-lane paved roadway that traverses the southern portion of the site in a southwest to northeast direction. Valencia Road is designated as a Gateway Arterial Route by the City of Tucson Major Streets and Routes Plan. In the project area, Valencia consists of four lanes with a curbed median and outside curbs with no multi-use lanes and no sidewalks. There are some existing sidewalks on newly developed projects and on both sides of Valencia Road under Interstate 10.

Littletown Road is located along the southern border of the parcel intersecting with and southeast of Valencia Road. Littletown is a two-lane paved road without curbs or sidewalks and travels in an east to west direction and terminates at Van Buren Avenue.

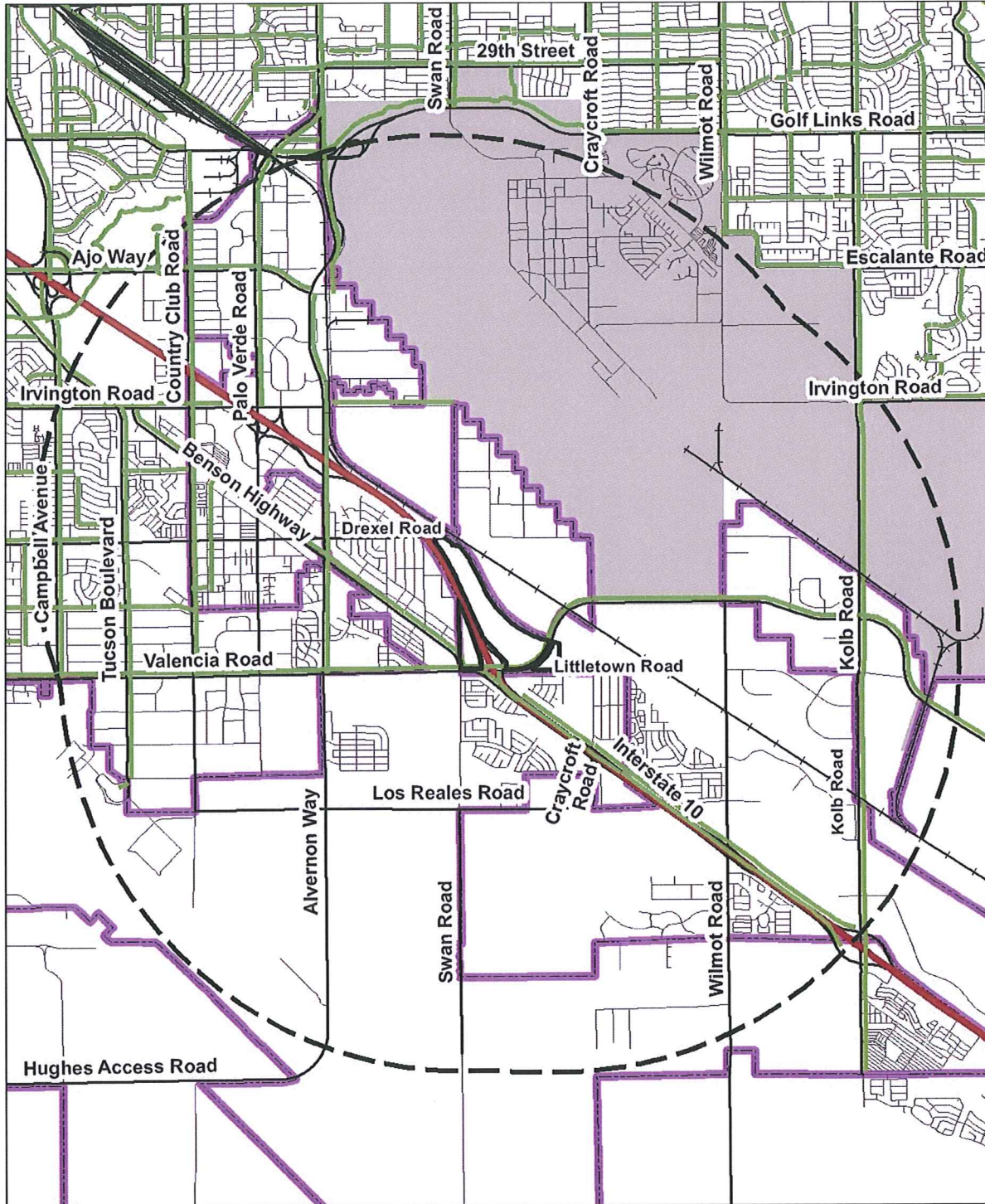
Drexel Road is located northwest and northeast of the site. Currently, Drexel Road ends at Belvedere Avenue at the west frontage of Interstate 10. Pavement, however, continues along the Drexel Road alignment, beneath a 14.4-foot highway overpass at Interstate 10 and terminates at the edge of the northwest corner of the site.

Access to the western parcels of the site can also be reached by Benson Highway which terminates at the parcel's western boundary. (*See Exhibit II.C.1.a: Roadways, page 15.*)

Exhibit II.C.1.a on page 15 shows major roads within a 3-mile radius of the property. *Table II.C.1 on page 16* indicates existing and future rights-of-way; rights-of-way width standards conformance; continuous rights-of-ways; travel lanes; road capacity; speed limits; existing pavement edge on existing roads and planned off-site streets for all major roads within a 3-mile radius of the property. *Exhibit II.C.1.b on page 17* also identifies the existing and future right-of-ways surrounding the site.

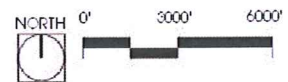


Exhibit II.C.1.a: Roadways



LEGEND

- Site Boundary
- Three-Mile Radius
- Bike Routes
- Davis Monthan Air Force Base
- Jurisdictions



FILE NAME: FTP-02_traffic.mxd
SOURCE: Pima County DOT GIS, 2011



Table II.C.1: Roadway Characteristics

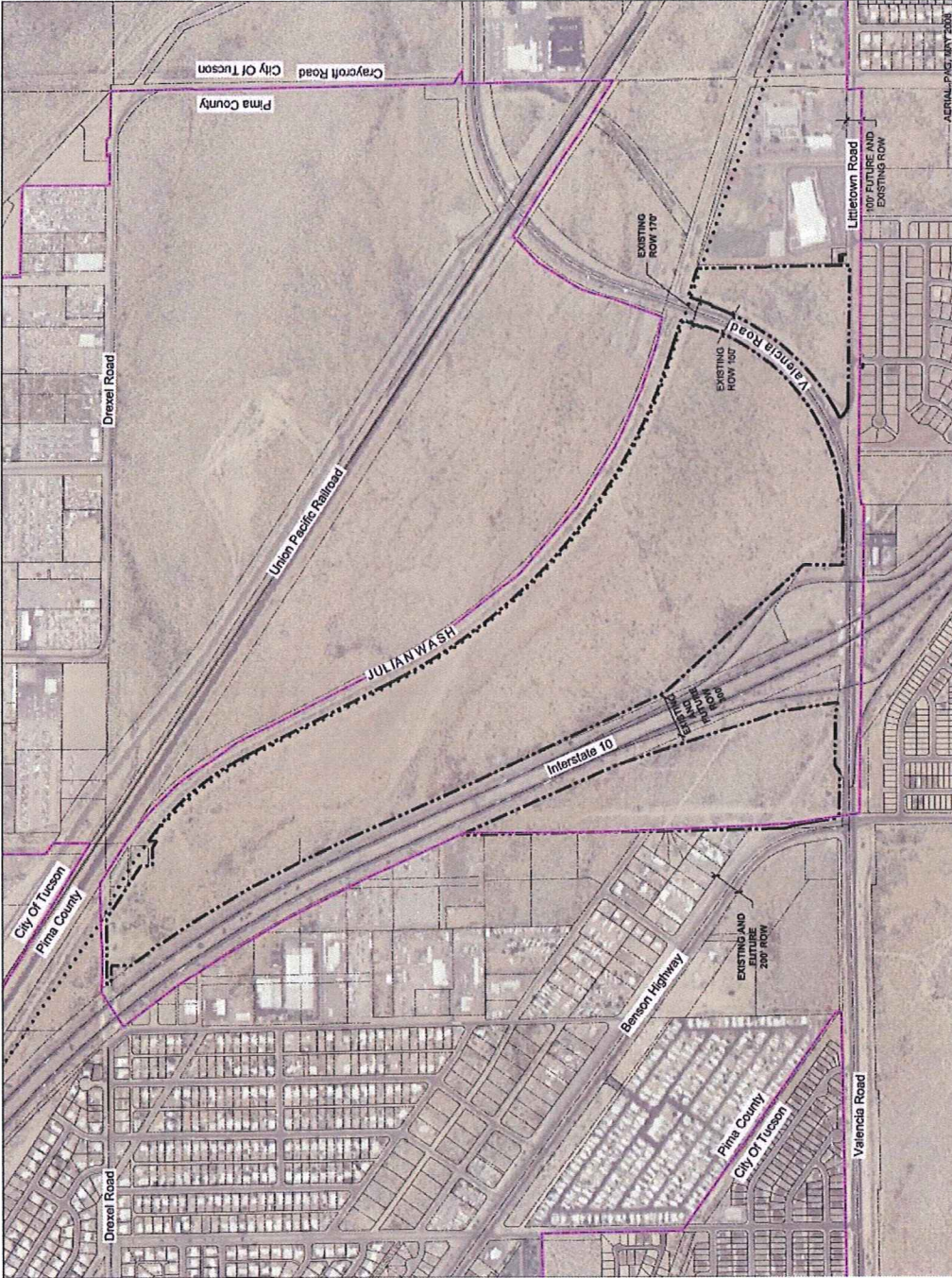
Road Name	Valencia Road	Interstate 10	Littletown Road	Benson Highway	Drexel Road
City of Tucson Classification	Gateway Arterial Route	State or Interstate Highway	N/A	Major Route	Major Route
Existing ROW	150' – 170'	300'	100'	200'	95' – 100'
Future ROW	200'	300'	100'	200'	100'
Divided	Yes	Yes	No	Yes	No
Paved	Yes	Yes	Yes	Yes	Yes
Continuous	Yes	Yes	Yes	Yes	No
Travel Lanes	4	4	2	4	2
Capacity	34,900	74,600	15,600	34,900	5,000
Speed Limit	50	65	35	45	35 to West 45 to East
Bike Route	Yes (only for experienced riders)	Yes (south of subject property)	No	Yes	No
ADT*	27,967 (2009)	53,000 (2009)	3,300 (2010)	7,900 (2009)	4,400 (2009)
Ownership	City of Tucson/Pima County	State of Arizona	City of Tucson/Pima County	City of Tucson/Pima County	City of Tucson/Pima County

* Average Daily Trip (ADT) Data taken from Pima Association of Government, Arizona Department of Transportation and Pima County Department of Transportation.



Exhibit II.C.1.b: Rights-of-Way

- Legend
- Site Boundary
- Julian Wash Trail
- Jurisdictional Boundary



2. Access Points

The site is currently accessed via Valencia Road, Drexel Road, Littletown Road or Benson Highway.

3. Scheduled Roadway Improvements

According to the Pima County Transportation Bond Improvement Plan and Pima Association of Government's (PAG) 2030 Regional Transportation Plan, there is a roadway improvement project planned for the section of Valencia Road between Interstate 10 and Houghton Road. The improvements would widen Valencia Road from four lanes to six lanes. In addition, Valencia Road will be improved in front of Desert View High School, where median, signal and parking improvements are planned and bonded.

ADOT has on-going plan for a Design Concept Report to identify the demands from I-19 to Cochise County line.

4. Bicycle and Bus Routes

Bicycle Routes exist along Valencia Road, Interstate 10 (south of the subject property), Alvernon Way and Benson Highway, as shown on *Exhibit II.C.4: Bicycle and Bus Routes*, page 19.

Exhibit II.C.4 also shows all bus routes within a one-half mile radius of the property. SunTran's bus route #26-Benson Highway nearly circumnavigates the site running along the western and southern boundaries. SunTran Route #26 runs along Drexel, Belvedere, Benson Highway, Valencia and Littletown Roads in the project area. The nearest bus stops are located west of the project site at the corner of Valencia and Swan Roads and southeast of the site on Craycroft and Littletown Roads.

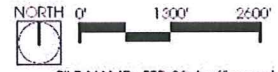


Exhibit II.C.4: Bicycle and Bus Routes



LEGEND

- Site Boundary
- One-Half-Mile Radius
- Benson Highway Bus Route #26 (Sun Tran)
- Bicycle Routes



FILE NAME: FTP-02_traffic.mxd
SOURCE: Pima County DOT GIS, 2010



D. Hydrology

1. Off-Site Watersheds

There are five off-site watersheds that affect the parcels east of Interstate-10 (I-10). Four of the five watersheds are significant and one is minor (See *Exhibit II.D.1: Off-Site Watersheds*, page 21.) The first off-site watershed (OS-1) is the Julian Wash, with a flow of 3357 cfs, which borders the northeastern edge of the site. The second Off-Site Watershed, OS-2 impacts the site along the east side of Valencia Road near its intersection with Littletown Road. The flow for this watershed, 208 cfs, was obtained from the plat for the Empire Vista subdivision south of the site (P1203-13). Off-Site Watershed OS-3 impacts the site near the intersection of Valencia Road and the Westbound I-10 on-ramp. It has a flow of 38 cfs, which is shown on Development Plan P1207-049. OS-4 impacts the site along the I-10 on-ramp, approximately 1,000 feet downstream of OS-3. Its flow is about 50 cfs. The minor off-site watershed that impacts the parcels east of I-10 is the nuisance runoff from the northern half of I-10. The cumulative flow is about 40 cfs.

For the parcel west of I-10, the only off-site watershed is the nuisance runoff from the southern half of I-10. The cumulative flow is about 75 cfs over the length of the entire I-10 frontage. The significant off-site flows are intercepted by Valencia Road and I-10.

2. Off-Site Natural or Man-Made Features

The Julian Wash (OS-1) is a manmade, earthen channel, which flows from southeast to northwest along the northern property boundary. The Julian Wash channel is generally trapezoidal in shape with depths of 10 to 12 feet. Although the channel was originally manmade, it has many features of a natural channel such as a sandy bottom and mature native vegetation along the banks. OS-2 enters the site via five, existing 30-inch RCP's under Littletown Road. The runoff flows through the site in an existing roadside channel along the southeast side of Valencia Road, ultimately discharging into the Julian Wash. OS-3 enters the site via three, existing 30-inch reinforced concrete pipes (RCP) under Valencia Road. The runoff then flows through the site along the northeast side of I-10. OS-4 enters the site via an existing, 30-inch RCP and an existing, 24-inch CMP under the I-10 on-ramp where it combines with the runoff from OS-3.



Exhibit II.D.1: Off-Site Watersheds



3. Upstream Off-Site Watersheds

The reported peak discharges were either taken from approved studies, reports and plans, or calculated based on City of Tucson Methodology. Per the Effective Flood Insurance Study (FIS), the 100-year discharge in the Julian Wash (OS-1) is 3,357 cfs, which is contained in the channel. Per the Final Plat for Empire Vista, Lots 1-153, the 100-year discharge for OS-2 is 208 cfs. Per the Development Plan for Jack-in-the-Box, the 100-year discharge for OS-3 is 38.4 cfs. The 100-year discharge for OS-4 is approximately 50 cfs while approximately 40 cfs is generated from the northern half of I-10.

The parcel south of I-10 is impacted by approximately 75 cfs generated from the southern half and median of I-10.

4. On-Site Hydrology (East of I-10)

The on-site hydrology is described in two sections, one for the portion of land west of I-10 and one for the portion of land east of I-10. (See *Exhibit II.D.4: On-Site Hydrology*, page 23.) The following information relates to the site located east of I-10.

a. 100-Year Floodplains

There are two off-site regulatory watercourses that impact the site. The first is the Julian Wash, which is contained in the channel. Runoff from OS-2 is contained in the channel paralleling the southeast side of Valencia Road. The combination of runoff from on-site watershed 2 and off-site watersheds OS-3, OS-4, and the northern half of I-10, results in a discharge greater than 100 cfs. This runoff flows through the site in a shallow sheet flow nature, ultimately discharging into the Julian Wash.

b. Sheet Flooding Areas

Although not mapped as regulatory sheet flood areas, much of the site is impacted by sheet flow. See discussion above.

c. Federally-Mapped Floodways and Floodplains

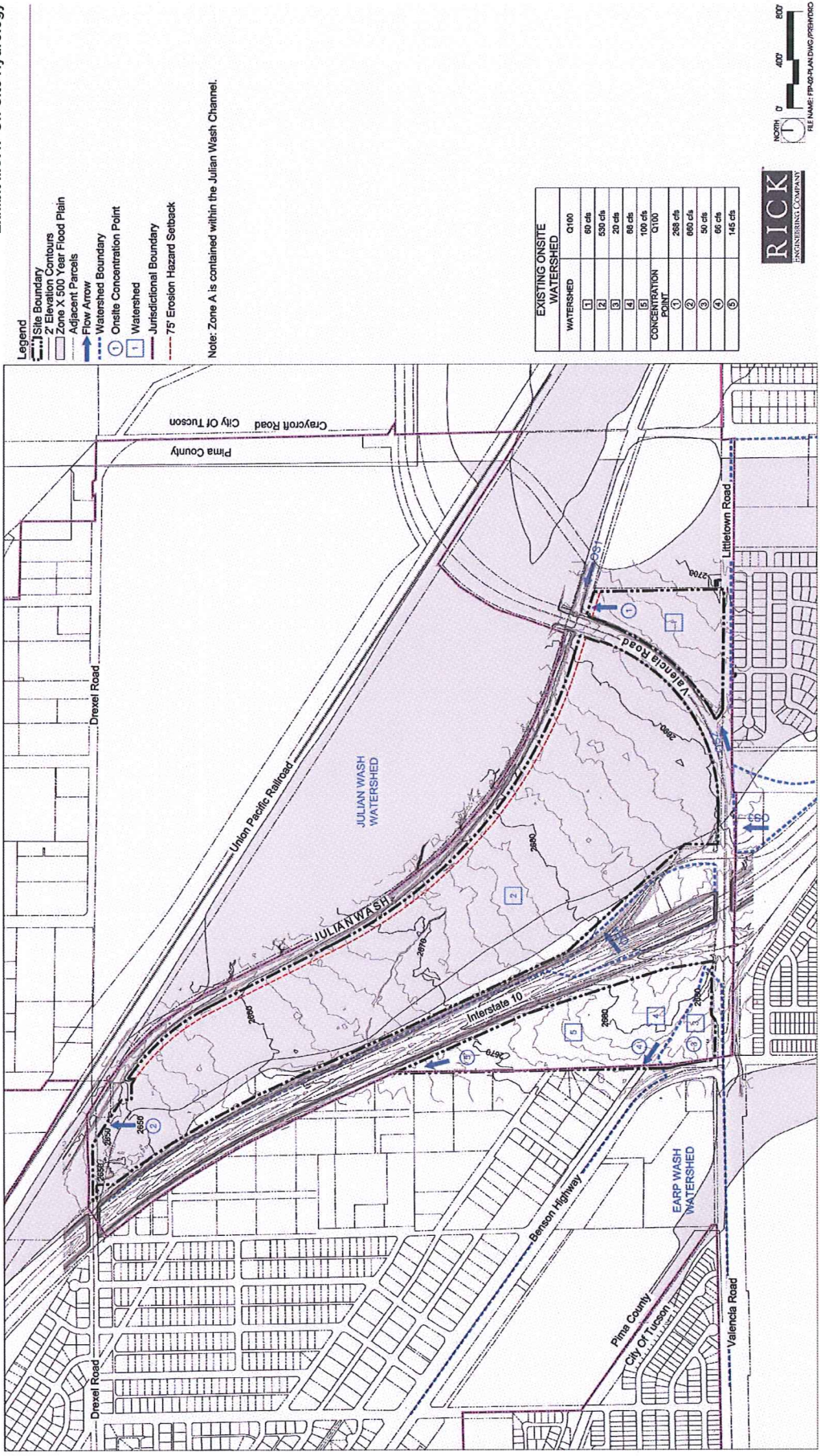
As shown on the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map (FIRM) 04019C2245K, revised by Letter of Map Revision (LOMR) Case No: 99-09-1084P, effective June 16, 2000, the area of the site is located in a Shaded Zone X, defined as areas subject to the 500-year flood. The Julian Wash is a Zone A –100 year floodway that is completely contained in a channel.

d. Peaks Discharges

The existing 100-year peak discharge for on-site Watershed 1 is 60 cfs with a cumulative discharge of 268 cfs at Concentration Point 1. On-site Watershed 2 generates 530 cfs with a cumulative discharge of 660 cfs where Concentration Point 2 discharges into the Julian Wash.



Exhibit II.C.4: On-Site Hydrology



- e. Existing Drainage Infrastructure
As discussed in the previous sections, there are numerous culverts under I-10, Valencia Road and Littletown Road that allow runoff to enter the site. There is also a constructed channel on the southeast side of Valencia Road that conveys flows through Watershed 1 to the Julian Wash.
- f. Perennial Surface Water
There are no lakes, ponds, wetlands, springs or other sources of perennial water on the site.
- g. Erosion Hazard Setbacks
Per the Standards Manual for the Drainage Design and Floodplain Management in Tucson, Arizona, the Erosion Hazard Setback is the square root of the flow. The setback for the Julian Wash (3557 cfs) is 58 feet. The channel in Watershed 1 (268 cfs) has a setback of 16 feet while the channel in Watershed 2 (530 cfs) has a setback of 23 feet. Erosion hazard setbacks may be reduced with the use of adequate bank protection.

However, In addition, final erosion hazard setback area from any regulatory watercourse must be determined by a separate Drainage Report specific to the watercourse in question at time of development plan submittal. All requirements within DS Sec.10-02.7.6.1 for determining the erosion hazard setback from a regulated watercourse will be required at time of submittal
- h. Environmental Resource Zone/WASH Ordinance Wash
The Julian Wash is a WASH Ordinance wash. A minimum 50-foot setback is required from top of bank.

5. On-Site Hydrology (West of I-10)

The on-site hydrology is described in two sections, one for the portion of land west of I-10 and one for the portion of land east of I-10. (See also *Exhibit II.D.4: On-Site Hydrology*, page 23.) The following information relates to the site located west of I-10.

- a. 100-Year Floodplain
When combined with off-site runoff, Watersheds 5 has a discharge greater than 100 cfs. This watershed has dispersed flow exiting the site along much of the western property boundary.
- b. Sheet Flooding Areas
There are no mapped regulatory sheet flood areas.



- c. **Federally-Mapped Floodways and Floodplains**
As shown on the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map (FIRM) 04019C2245K, this area of the site is located in an Unshaded Zone X, defined as areas outside of the 500-year flood.
- d. **Peak Discharges**
The existing 100-year runoff generated from Watershed 3 is 20 cfs with a cumulative discharge exiting the site of 50 cfs. Watershed 4 has a discharge of 66 cfs. Watershed 4.5 generates 100 cfs on site with a cumulative discharge of 145 exiting the site.
- e. **Existing Drainage Infrastructure**
There are no existing drainage improvements on this portion of the site.
- f. **Perennial Surface Water**
There are no lakes, ponds, wetlands, springs or other sources of perennial water on the site.
- g. **Erosion Hazard Setbacks**
The erosion hazard setback for Watershed 5 on the parcel west of I-10 is 12 feet. However, final erosion hazard setback area from any regulatory watercourse must be determined by a separate Drainage Report specific to the watercourse in question at time of development plan submittal. All requirements within DS Sec.10-02.7.6.1 for determining the erosion hazard setback from a regulated watercourse will be required at time of submittal

6. Existing Drainage Conditions along the Downstream Property Boundary

- a. **East of I-10**
Downstream of the eastern parcels of the property are constructed channels: the Julian Wash for Watershed 2 north of Valencia Road and the channel along the southeast side of Valencia Road, which intersects the Julian Wash for Watershed 1 south of Valencia Road. These channels contain the flows leaving the site.
- b. **West of I-10**
A swale and culvert under Benson Highway along the north side of Valencia Road convey the flows from Watershed 3 to the west. A swale along the north side of the Benson Highway conveys the flow from Watershed 4 to the northwest. There are no downstream improvements to convey flows from Watershed 5 in the northern section of the western parcel. The flow drains to the north until it is redirected to the northwest by I-10 through large industrial parcels.



E. Vegetation

1. Pima County Xeroriparian Habitat

Pima County Mapguide indicates the existence Xeroriparian Habitat D on the site as shown on *Exhibit I.E.1: Riparian Habitat*, page 27. These areas are isolated and there is no connectivity between higher classes of Xeroriparian Habitat. As a result, the Xeroriparian Habitat D is considered Low Habitat, and the site's value to wildlife habitat is diminished by the surrounding land uses which isolate the site from wildlife movement. The site is entirely bounded by I-10 to the west, Valencia Road to the south and east, and the Union Pacific rail line to the north and east. I-10 serves as the eastern and western boundary of the project and creates a barrier to wildlife movement.

2. Vegetative Communities

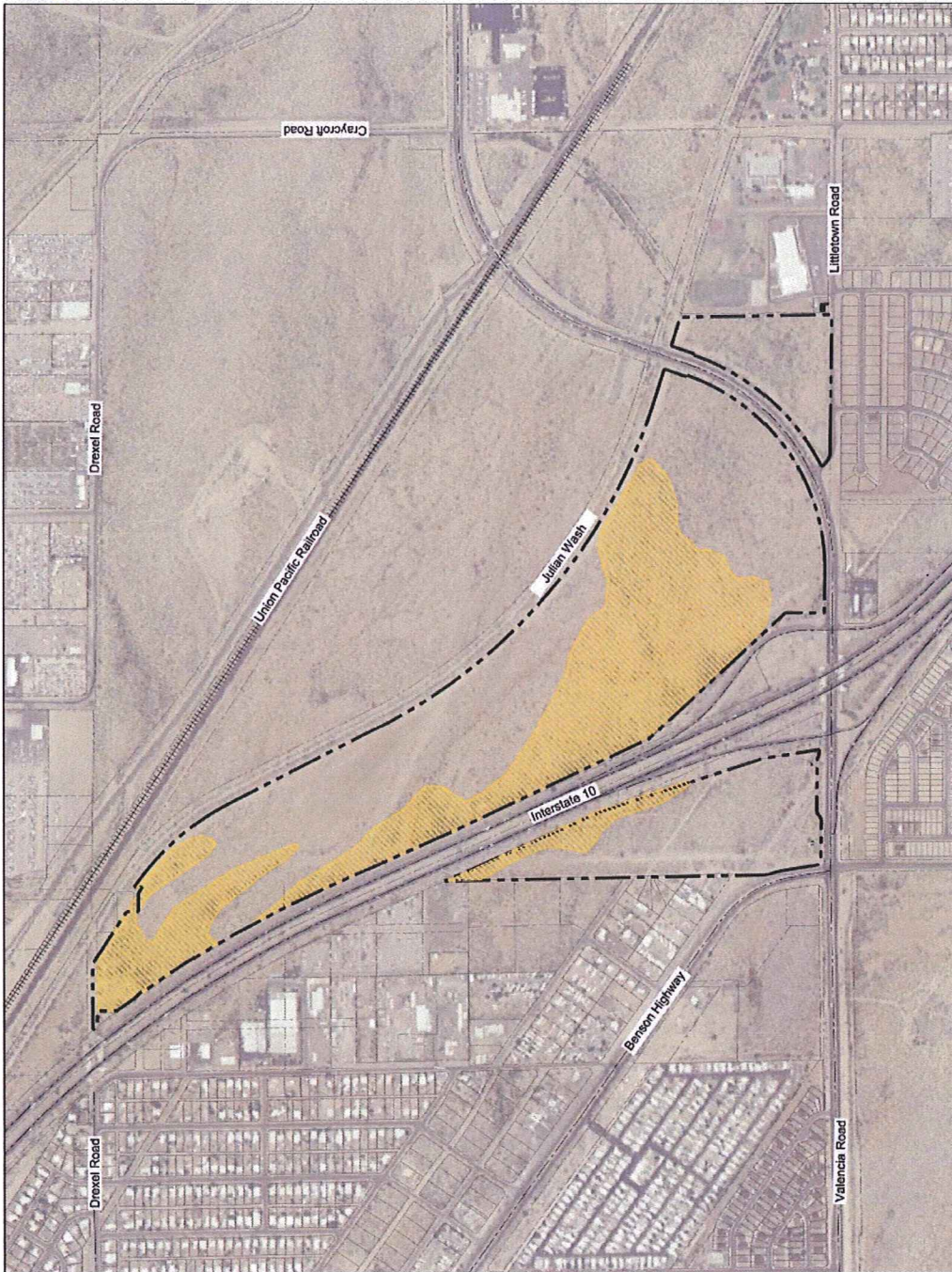
As shown in Photo 1, the portion of the site west of I-10 is nearly a monoculture of Creosote bush consisting almost entirely of Creosote and Cholla, with only a few Mesquite trees. The areas directly adjacent to I-10 contain a large number of Cholla cactus, increased Creosote density and a few Mesquite trees.



Photo 1: View looking north from the eastern boundary of the parcel west of Interstate 10.

Exhibit II.E.1: Riparian Habitat

- Legend
- Site Boundary
 - Existing Prime County Mapped Xeroriparian "D" Area



Source: PAG, 2008

The portion of the site east of I-10 contains more vegetative diversity with large areas covered with Sonoran scrub. The southern portion of the site contains an area with large numbers of Cholla and Prickly Pear cacti. This area contains small numbers of Barrel cacti, Mesquite trees and Creosote bush. The area directly adjacent to I-10 contains an increased density of vegetation.

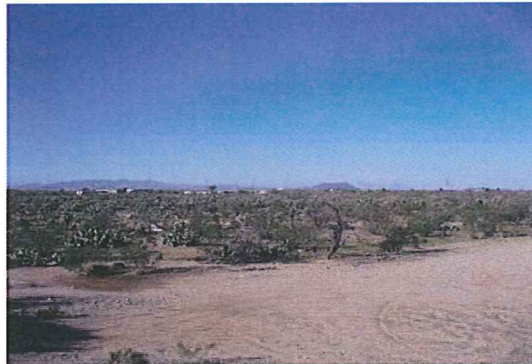


Photo 2: View (east of Interstate 10) looking west on the north side of Valencia Road at the southeast corner of the property.

The far northern portion of the site is highly disturbed by earlier grading, dumping, and off-road vehicle use. This area does contain a small number of larger Mesquite trees. However, the high level of disturbance has removed any smaller or understory vegetation. The northern portion of the site contains vast areas of Creosote bush. A large area in the center of the property is nearly or totally devoid of vegetation. A second centrally located area contains dead or dying Mesquite trees and shrubs.

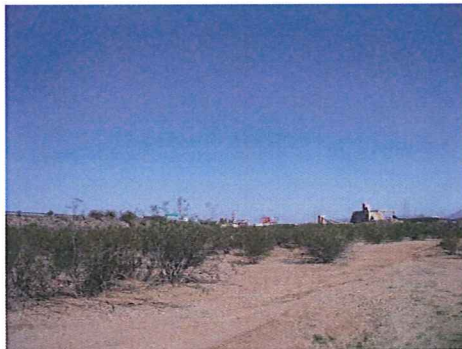


Photo 3: View looking north at the northern portion of the site (east of Interstate 10).



Photo 4: View looking southwest from the northern portion of the site (east of Interstate 10).

F. Site Photos



Exhibit I.F.2: Site Photos on page 31 displays photographs taken from different angles of the site. The approximate locations from which these photographs were taken are displayed on *Exhibit I.F.1: Photo Key Map*, page 30.



Exhibit II.F.1: Photo Key Map



LEGEND

-  Site Boundary
-  Photo ID & location photo was taken



FILE NAME: ATC-03_photokeymap.mxd
SOURCE: Pima County DOT GIS, 2009



Exhibit II.F.2: Site Photos



Photo 1: From the existing gas station located south of the project site looking northeast toward the site along Valencia Road.



Photo 2: From the south boundary of the project site looking north.



Photo 3: From the southeastern corner of the project site along Littletown Road looking east/northeast toward the existing school site and Catalina Mountains.



Photo 4: From the northern portion of the site looking north toward adjacent land uses.



Photo 5: From the northern portion of the site looking east toward the Union Pacific Railroad.



Photo 6: Looking north from the southwest corner of the project site.



G. Utilities

1. Wastewater

a. PCRWRD Capacity Letter

Currently, there is a 30-inch line (G-84-40) in the Julian Wash located along the northern boundary of the site and an 8-inch diameter sewer located in Valencia Road. The 30-inch sewer line extends west along Drexel Road under Interstate 10.

A proposed augmentation of the existing sewer system is planned for completion in 2011; thus, capacity will be available to serve this project prior to its development. The site is tributary to Roger Road Water Reclamation Facility via the Southeast Interceptor and Northwest Outfall. (See *Exhibit II.G.1: PCRWRD Capacity Letter*, page 33.)

b. Locations of Existing Public Sewers in Relation of the Project Site

As indicated in *Exhibit II.G.2: Existing Sewer Network* on page 34, there is a 30-inch (G-84-40) and an 8-inch (G-99-115) public sewer line adjacent to the site. The 30-inch line is located adjacent to the Julian Wash along the northern boundary of the site, and the 8-inch line is within the Valencia Road right-of-way bisecting the site along the southern boundary. The 30-inch line extends along Drexel Road under Interstate 10.

2. Water

Tucson Water and Ray Water Company provide service to properties in the vicinity of the project. The site is located within the City of Tucson obligated water service area. In order to obtain water service to the site, the property has been annexed into the City of Tucson. A water service agreement will be required to establish service to the property.



Exhibit II.G.1: PCRWRD Capacity Letter



**Pima County
Regional Wastewater Reclamation Department**

Michael Gritzuk, P.E.
Director

201 N. Stone Ave., 8th Floor
Tucson, Arizona 85701
(520) 740-6500

Visit our website:
<http://www.pima.gov/wwm>

August 23, 2010

Kelly Lee
The Planning Center
110 S. Church, # 6320
Tucson, AZ 85701

Capacity Response No. 10-019 Type I

**RE: Valencia & I-10 Specific Plan, Parcels # 140-34-0690 thru 140-340-690.
Estimated Flow 228,000 gpd (ADWF).**

Greetings:

The above referenced project is tributary to the Roger Road Water Reclamation Facility via the Southeast Interceptor and Northwest Outfall.

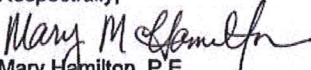
Capacity is currently available for this project in the 30-inch public sewer G-84-40 and the 8-inch public sewer G-99-115, adjacent to the property.

This letter is not a reservation or commitment of treatment or conveyance capacity for this project. It is an analysis of the system as of this date and valid for one year.

Note: Conditions within the public sewer system constantly change. An update to this letter must be obtained to verify that capacity exists in the downstream public sewer system just prior to submitting the development plan or subdivision plat for review and approval.

If further information is needed, please feel free to contact us at (520) 740-6500.

Respectfully,

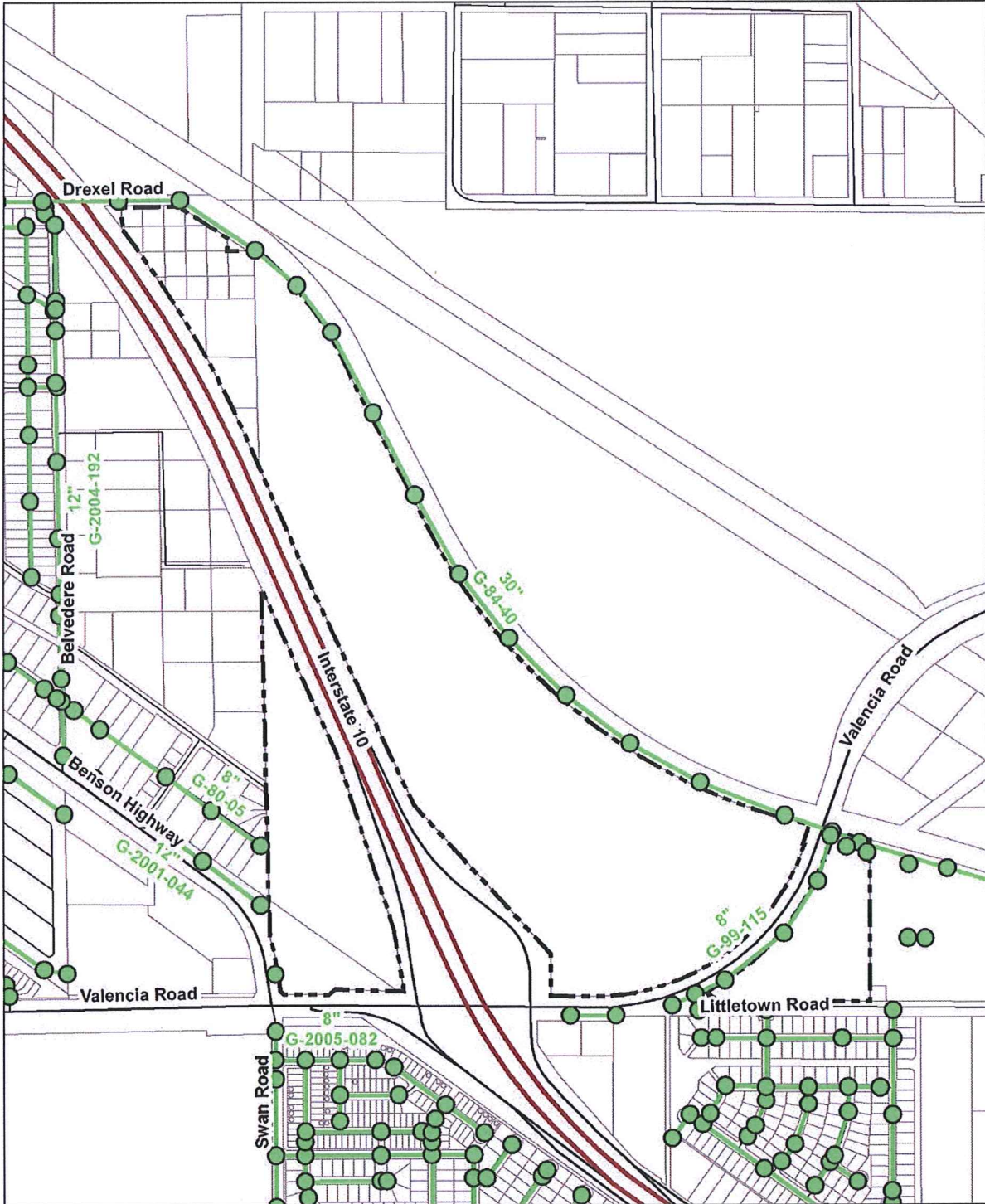

Mary Hamilton, P.E.
PCRWRD Planning Section Manager

MH:ks

c: Subhash Raval, DSD; T15, R114, Sec. 10 & 11




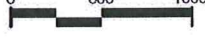
Exhibit II.G.2: Existing Sewer Network



LEGEND

-  Site Boundary
-  Manhole Covers
-  Sewer Network with Pipe Diameter and As-Built Identification Number

NORTH 



FILE NAME: FTP-02_sewer.mxd
SOURCE: Pima County DOT GIS, 2010



H. Cultural Resources

1. Arizona State Museum Letter

According to the Arizona State Museum (*Exhibit II.H: Arizona State Museum Letter*, page 36), there have been surveys that include the project site. One cultural resource, SR 80, has been identified within the subject property area. Eleven cultural resources were recorded within a one-mile radius of the parcels; all were historic linear features, including roads, railroads, or gas pipelines with the exception of one prehistoric site that was destroyed during the construction of Interstate 10.

2. Map and Description of Archaeological or Historic Sites

One cultural resource was found within the project site. The location of this site is State Route 80, a modern road whose segment within the project area is likely not historically important. A cultural resources survey was conducted on the property by Professional Archaeological Services and Technologies (PAST) in July 2000. The fieldwork consisted of an on-the-ground survey where field personnel were spaced 20 meters apart and crossed the subject property in a series of contiguous corridors. There were no surface indications of archaeological resources which meet the minimum standards for recording as an archaeological site or that would be eligible for inclusion in the National Register of Historic Places. Based on the field methods employed and the observable surface indications, there does not appear to be sufficient evidence to warrant further archaeological studies on the property. However, in the event that cultural resources are revealed during ground-disturbing activities, all construction shall cease, and consultation shall be initiated with ASM to assess potential significance of any unearthened materials (ARS 41-841). If human skeletal remains or funerary objects are discovered, ASM will be contacted immediately (ARS 41-865 & 41-844). The archaeological report has been submitted under separate cover.

3. Field Survey Requirements/Results

The ASM defers to City of Tucson regarding recommendations concerning cultural resources requirements prior to any ground disturbing activities. Based on the results of the ASM site record check, the City of Tucson Historic Preservation Office may recommend that an on-the-ground survey be conducted by a qualified archaeologist prior to any ground disturbing activities.



Exhibit II.H: Arizona State Museum Letter



Arizona State Museum

P.O. Box 210026
Tucson, AZ 85721-0026
Tel: (520) 621-6302
Fax: (520) 621-2976

MAR 29 2010

PIMA COUNTY ARCHAEOLOGICAL RECORDS SEARCH RESULTS

E-mail Request Received: 2/19/2010

Search Completed: 3/17/2010

Requester Name and Title: Kelly Lee, Project Manager
Company: The Planning Center
Address: 110 South Church, Suite 6320
City, State, Zip Code: Tucson 85701
Phone/Fax/or E-mail: (520) 623-6146

Project Name and/or Number	Project Description
FTP-02 / 28 Parcels owned by Stewart T&T	Rezoning (multifunctional use) of about 189 ac

Project Location: Between Drexel Rd & Valencia Rd at I-10, Pima County, Arizona.

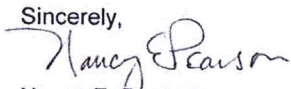
Legal Description: A portion of the W½ & SW, S11, and the E½, NE, S10, T15S, R14E, G&SR B&M, Pima County, Arizona.

Search Results: A search of the archaeological site records maintained at the Arizona State Museum (ASM) identified one cultural resource, SR 80, within the subject project area. The project area has been entirely inspected for cultural resources by several projects in the late 1980s and early 2000s. Eleven cultural resources are recorded within a mile radius of the parcels; all are historic linear features – roads, railroads, or gas pipelines – except for one, a prehistoric site that was destroyed during the construction of I-10. Fifty archaeological projects have been completed within the project area or within a mile of it between 1980 and 2006. A 2008 color orthophotograph taken of the proposed project area, enclosed, shows a mostly undisturbed landscape. Native vegetation covers most of the project area.

Sites in the Project Area: None are identified.

Recommendations: Because Pima County has jurisdiction in these parcels, the county's cultural resources staff bases its recommendations for additional archaeological inquiry on its own data sources, including the ASM's search results and others. Should the county require further archaeological investigation, you will need to consult a professional contractor. A list of qualified contractors is posted on the ASM website at the following address:
<http://www.statemuseum.arizona.edu/crservices/permits/permittees.shtml>.

If you have any questions regarding the results of this records search, please contact me at the letterhead address or at the phone number or email address as follows.

Sincerely,

Nancy E. Pearson
Assistant Permits Administrator
(520) 621-2096 Phone/FAX
nepearso@email.arizona.edu



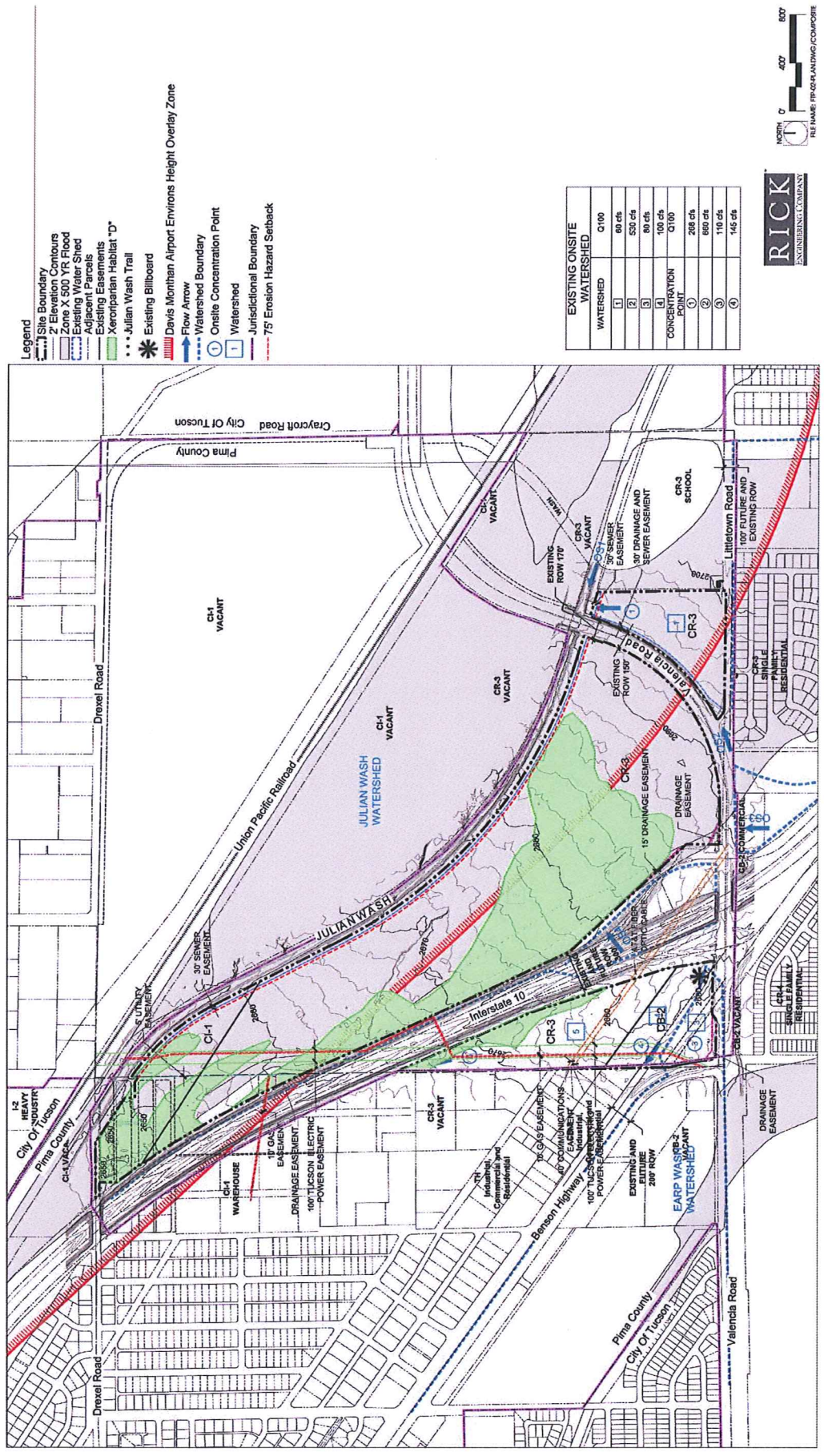
I. Composite Map

The composite map graphically illustrates the summation of constraints identified during the inventory and analysis process. The significant constraints are shown on *Exhibit II.I: Composite Map*, page 38, which include:

- Valencia Road is designated as a Gateway Route within the Gateway Corridor Zone. The purpose of this overlay zone is to implement policies in the City's General Plan, the Regional Transportation Plan, and the Air Quality Plan, with specific emphasis on the Major Streets and Routes (MS&R) Plan. These goals are achieved by providing for the visual improvement of major streets and routes designated as Gateway Routes by implementing standards for the design and landscaping of the roadway and adjacent development.
- FEMA FIRM Zone X (shaded) is located on the portion of the site east of Interstate 10; the remainder of the site is within an unshaded Zone X. (FEMA defines unshaded Zone X as areas of minimal flooding.) Shaded Zone X is defined as a 500-year flood, 100-year flood areas with average depths of less than one foot or with drainage areas less than one square mile and areas protected from 100-year floods by levees. Moreover, the FEMA FIRM maps indicate that the 100-year flood is contained within the Julian Wash channel. Therefore, the site is located within a FEMA mapped 500-year floodplain (FEMA/FIRM Panel is 2245, Community Number 04019C2245 K).
- The Julian Wash is designated as a Watercourse Amenities, Safety, and Habitat (WASH).
- Portions of riparian habitat are designated along the western boundary of the site adjacent to Interstate 10. However, the site's value to wildlife is diminished by the surrounding land uses which isolate the site from wildlife movement. The site is entirely bounded by I-10 to the west, Valencia Road to the south, and the Union Pacific rail line to the north and east.
- Davis Monthan Airport Environs Height Overlay Zone is located on the eastern portion of the site. The height limitation for this area is a maximum of 120 feet from finished grade.
- Several easements transverse the property from north to south: Tucson Electric Power transmission Lines, gas easements, communications easement, drainage easements and sewer easements. Existing easements to remain will be set aside from development and landscaping provisions will be applied.
- Interstate 10 bisects the property from northwest to southeast. Valencia Road is located along the south of the subject property and Drexel Road runs through an existing subdivision under Interstate 10 and ends at the northwest property corner. This road configuration limits access to the parcel west of Interstate 10 as access will be limited to Benson Highway and the northern portion of the property west of I-10.

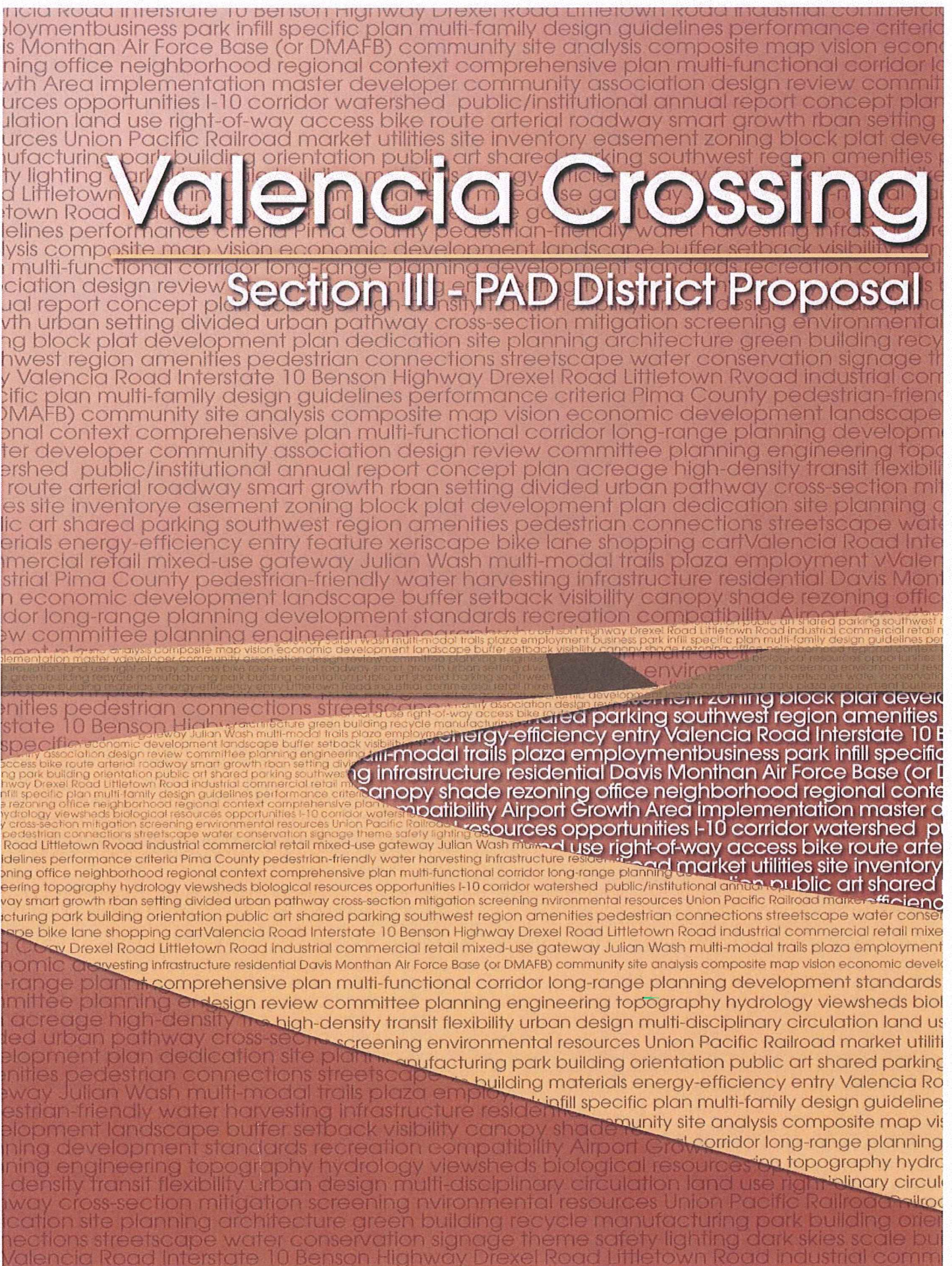


Exhibit II.i: Composite Map



Valencia Crossing

Section III - PAD District Proposal



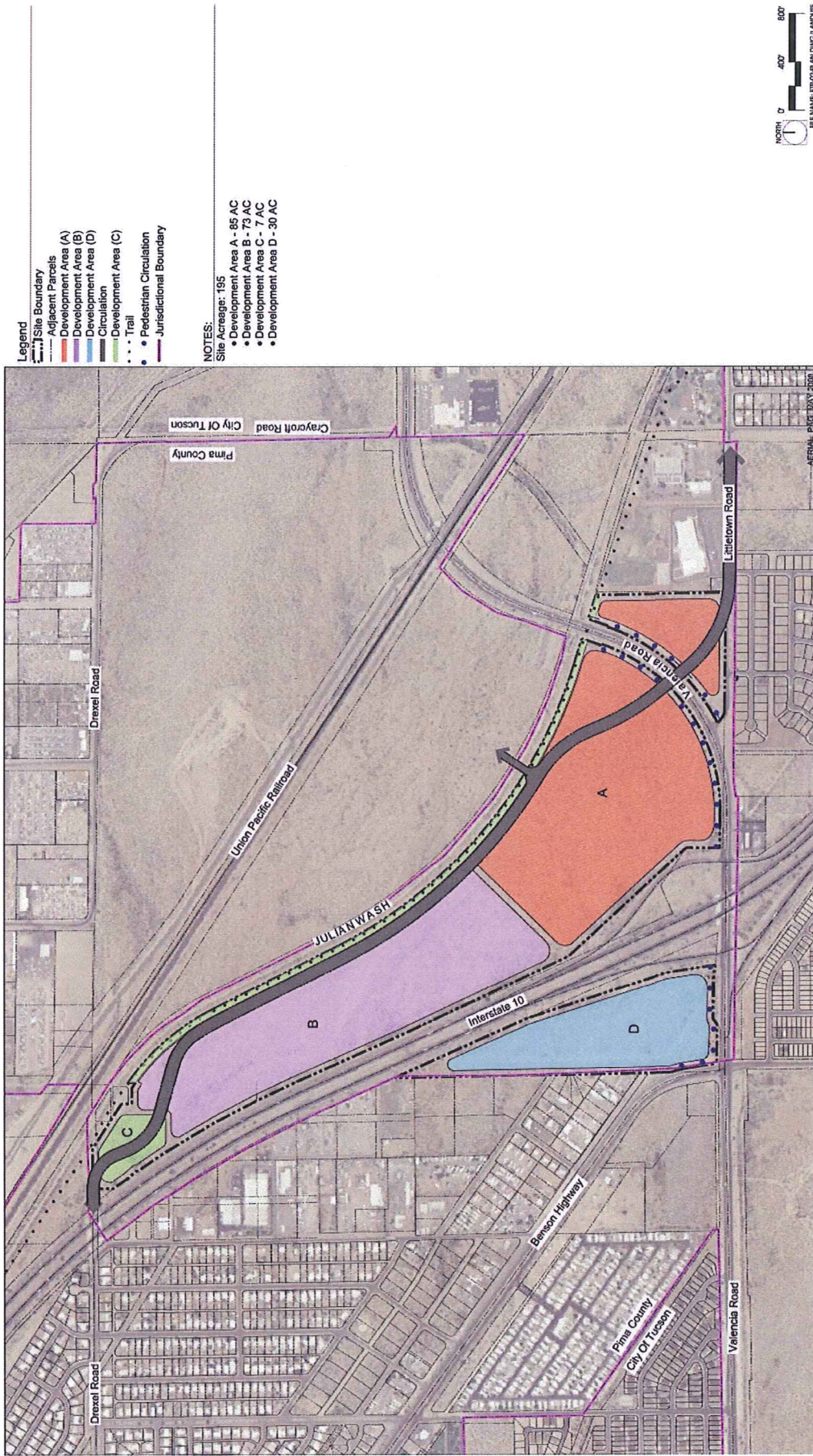
A. PAD Overview

The goal of the Valencia Crossing PAD is to provide high-quality complementary uses along Interstate 10—a community where people can live, work, play and shop. As shown on *Exhibit III.A: Land Use Concept Plan* (page 41), there are four development areas planned for the site, Development Area A, B, C and D. The following provides a description for each of these development areas:

- **Development Area (A):** This designation encompasses approximately 85 acres at the intersection of Valencia Road and the realigned Littleton Road. The primary use is anticipated for commercial development that will respond to the needs of the community with a blend of retail, entertainment and office space. Commercial uses envisioned within the designation of this PAD include large-scale retailers, an assortment of dining establishments, retail shops and office tenants. This location is ideal to cater to nearby residents with neighborhood-level uses such as a grocery store, pharmacy, post office or bank, as well as residents of outlying areas. Interstate 10 will make the commercial area easily accessible to residents in the immediate vicinity and commuters ranging from Downtown Tucson to areas southeast of Tucson and Vail.
- **Development Area (B):** Situated in proximity to the existing industrial and residential development on the west side of Interstate 10, this designation on the east side of Interstate 10 is envisioned to complement these uses with mixed use center consisting of a future business park, industrial, commercial as well as high-density residential uses.
- **Development Area (C):** The large detention basin located on the north end of the site is designed to be a dual-use facility, which detains and stores stormwater runoff while at the same time may provide for recreational/open space areas and uses such as communication facilities, utility generation, transmission, and/or distribution of basic services, and public facilities, such as neighborhood centers and community centers. It has not yet been determined that recreation will occur within this 7-acre area; however, this category also includes the 50-foot wide multi-use trail located along the Julian Wash.
- **Development Area (D):** Industrial and Commercial uses are anticipated west of Interstate 10 at Benson Highway and Valencia Road. The proposed uses may include, but are not limited to, office/incubator space, self-storage units, manufacturing and RV storage. Access will be provided from Benson Highway.



Exhibit III-A: Land Use Concept Plan



B. Permitted and Excluded Uses

1. Permitted Uses

a. Development Area A

Uses permitted within Development Area A shall include all permitted and special exception uses within the C-2 zone of the LUC, except as modified in this PAD. The following land uses and modifications shall apply.

- General manufacturing
- Precision manufacturing
- Large Retail Establishments as set forth in Section 3.5.9.7 of the LUC are permitted within Development Area A, as are all permitted and special exception uses in the C-2 Zone. Notwithstanding any specific provisions of such Section or of the LUC to the contrary, the following provisions shall govern Large Retail Establishments and no further public process is required:
 - All proposed Large Retail Establishments shall be reviewed and approved in accordance with the Development Plan Review process. Because the PAD District is established through the Zoning Examiner Legislative Procedure, no separate or additional approval through Special Exception Land Use, Zoning Examiner Legislative Procedure or otherwise, will be required for any Large Retail Establishment proposed in Development Area A other than approval of the Planning and Development Services Department Director.
 - Permitted Sales- General Merchandise Sales with Food and Beverage Sales may be combined within a single establishment with no limitation on gross floor area allocated to either one of the land use classes.
 - Buffers- The project shall provide visual and noise buffers where the site is adjacent to a residential use or residentially zoned property outside of the PAD boundaries. This can be accomplished by providing a minimum building setback of at least one hundred (100) feet. To avoid isolating Sub-Area A from rest of the PAD Development within Sub-Area A the screen walls and landscape buffers requirements in LUC 3.5.9.7.A.2 are inapplicable. Screen walls and landscape buffers for commercial sub-areas are set forth in Section D: Landscape Borders and Screening (pages 49-50).
 - Outdoor Storage- The project shall mitigate visual and noise impact on residential uses and residentially zoned properties by locating these areas on-site and at least one hundred (100) feet from any residential use or residentially zoned property outside of the PAD boundaries. In addition, a 6-foot screen wall is required adjacent to any residential use or residentially zoned property outside the PAD boundaries, the Julian Wash Greenway and adjacent public roadways unless the outdoor storage is already substantially screened by buildings or other landscaping. .



- Trash Collection- Trash collections behind structures adjacent to the Interstate 10 are allowed at any time, subject to the City of Tucson's Noise Ordinance. Trash collection locations will be allowed between 7:00 am and 7:00 pm in all other areas where a Large Retail Establishment may be located. The minimum setback shall be one hundred (100) feet from any residential use or residentially zoned property outside of the PAD boundaries, unless such operations are located entirely within an enclosed building, provided it is no closer than the allowable building setback. In addition, a 6-foot screen wall is required adjacent to any residential use or residentially zoned property outside the PAD boundaries, the Julian Wash Greenway and adjacent public roadways unless the outdoor storage is already substantially screened by buildings or other landscaping.
- Delivery and Loading- Delivery or Loading behind structures adjacent to I-10 will be allowed at any time, subject to the City of Tucson's Noise Ordinance. Delivery and loading will be allowed between 7:00 am and 10:00 pm in all other areas where a Large Retail Establishment may be located. The minimum setback shall be one hundred (100) feet from any residential use or residentially zoned property outside of the PAD boundaries, unless such operations are located entirely within an enclosed building, provided it is no closer than the allowable building setback. In addition, a 6-foot screen wall is required adjacent to any residential use or residentially zoned property outside the PAD boundaries, the Julian Wash Greenway and adjacent public roadways unless the outdoor storage is already substantially screened by buildings or other landscaping.
- Outdoor Lighting- metal halide lighting shall be permitted in the parking lot areas, provided that the lighting levels are reduced between 10:00 p.m. and 7:00 a.m. This modification is justified because metal halide lighting provides a high quality, cleaner, true spectrum light and allow for creative application landscape and pedestrian area feature lighting. Notwithstanding this modification, the outdoor lighting shall comply with the City of Tucson Outdoor Lighting Code.
- Outdoor Sales Display/Ancillary Uses- The minimum setback shall be one hundred (100) feet from any residential use or residentially zoned property that is adjacent to the PAD boundaries, provided it is no closer than the allowable building setback. Setbacks shall provide a noise barrier between any residential use or residentially zoned properties and the outdoor sales and/or ancillary uses. Screen walls and landscape buffers for commercial sub-areas are set forth in Section D. Landscape Borders and Screening (pages 49-50).
- Live/Work Unit, subject to:
 - No more than 75 percent of all building square footage on the lot may be devoted to the work unit.
 - One motor vehicle space is required per unit and bicycle parking is not required for live/work unit



b. Development Area B

Uses permitted within Development Area B shall include all permitted and special exception uses within the C-2 and I-1 zones of the LUC, except as modified in this PAD. The following land uses and modifications shall apply:

- Open Space
 - Typical uses include, but not limited to, unimproved land, that is set aside, dedicated, or reserved in perpetuity for public or private enjoyment.
- Live/Work Unit, subject to:
 - No more than 75 percent of all building square footage on the lot may be devoted to the work unit.
 - One motor vehicle space is required per unit and bicycle parking is not required for live/work unit.

c. Development Area C

Development Area C permitted uses may include but not limited to:

- Communications Facilities
 - Typical uses include wireless communication towers.
- Drainage Facilities
 - Typical uses include, but not limited to, flood control facilities, such as drainage structures, water harvesting basins and stormwater detention/retention dams and basins.
- Neighborhood Recreation
 - Typical uses include, but not limited to, park areas, ball/soccer fields, volleyball/basketball courts, playground equipment, park amenities (ramadas, barbeques, and park benches) and vehicle parking used by the residents, employees and/or customers of Valencia Crossing PAD.
- Open Space
 - Typical uses include, but not limited to, unimproved land, that is set aside, dedicated, or reserved in perpetuity for public or private enjoyment.
- Parking
 - Vehicular parking areas to be limited to 20% of Area C.
- Public Utilities
 - Typical uses include, but not limited to, utility generation, solar, transmission, and/or distribution of basic utility services for uses, such as sanitation, water, gas, and electrical services.
- Trails
 - Typical Uses include, but not limited to, unpaved or paved paths permitted for use only by pedestrians and non-motorized bicycles.



d. **Development Area D**

Uses permitted within Development Area D shall include all permitted and special exception uses within the I-1 and C-2 zone of the LUC. The following land uses and modifications shall apply:

- **Billboards**
 - The single existing billboard (located on Development Area D) shall be removed within 370 days following submittal of the first development plan within the PAD boundaries. .
 - The public process for deviations from gateway standards shall be combined with the PAD public process zoning examiner legislative procedures.
- **Live/Work Unit, subject to:**
 - No more than 75 percent of all building square footage on the lot may be devoted to the work unit.
 - One motor vehicle space is required per unit and bicycle parking is not required for live/work unit.

2. Secondary Uses

Land uses secondary to the Permitted Land Uses are permitted within the PAD, subject to the requirements of LUC Section 3.2.4 and to any additional requirements listed with the use.

3. Accessory Land Uses

Land uses accessory to the Permitted or Secondary Land Uses are allowed within the PAD, subject to compliance of LUC Section 3.2.5.

4. Excluded Uses

The following uses shall be excluded from the Valencia Crossing PAD:

- All uses within the Restricted Adult Activities Use Group
- Trade Service and Repair- Major (except within Development Area D)
- Golf Course
- Billboards



C. Development Standards

For the purpose of setback requirements and other similar development standards that would otherwise apply to separately owned lots or parcels under the LUC and City Development Standards, the PAD shall be considered as a single parcel. Portions of the PAD that contain multiple parcels will be required to process lot combinations as part of development plan or plat approval. All new development within the PAD shall conform to applicable building, fire and other life safety standards, in addition to those development standards contained within Table III.C.

Table III.C: Development Standards

	Commercial and Industrial Uses	Residential Uses
Minimum Site Area	None	
Minimum Lot Width	None	
Maximum Lot Coverage	None	85 percent
Maximum Floor Area Ratio	None	
Maximum Building Height	120 feet from finished grade*	
Minimum Building Separation	Per Building Code	
Minimum Perimeter Setbacks	20 feet	
Setbacks from Non-Residential to Residential Uses (does not include mixed use developments, see definition, page 66)	<ul style="list-style-type: none"> 20 feet from any interior property line unless adjacent to non-residential uses 	

* Davis Monthan Airport Environs Height Overlay Zone covers a portion of the property east of Interstate 10. The height limitation for this area is a maximum of 120 feet from the elevation at the end of the runway. According to Davis Monthan officials, the runway elevation is approximately 2705 feet, and thus, higher than the elevation of the subject property. Therefore, the maximum building height is 120 feet from finished grade of the subject property. The elevation shall be indicated on every development plan and/or plat submitted for review to the City of Tucson. If the elevation is higher than 2705 the proposed building may not be 120 feet tall.



1. Demonstrated Parking

Parking stall counts shall be calculated across the Valencia Crossing as a whole, not necessarily on a parcel by parcel basis within the Development Area. As individual building permits are acquired, each permitted building must demonstrate that at least 80 percent of the required parking for that building is provided. The PAD will meet 100 percent of the required parking by the City of Tucson Land Use Code, Division 3, Section 3.3.4: Required Number of Motor Vehicle Parking Spaces, at the time of issuance of the last Certificate of Occupancy for the last new building to be built on-site. At the time of Development Plan review for Valencia Crossing PAD, the vehicular parking should be shown by using a comprehensive parking calculation table.

2. Bicycle Parking

The PAD will comply with the Bicycle Parking requirement of Division 3 of Article 3 of the LUC with the following exceptions:

a. Short-term Bicycle Parking

- Short-term parking shall be within seventy-five feet of a public entrance to a building as measured along the most direct pedestrian access route.
- Where buildings have more than one public entrance or a site has more than one building, short-term bicycle parking shall be distributed so that at least one short-term bicycle parking space is within seventy-five feet of each public entrance.

b. Long-term Bicycle Parking

- Long-term bicycle parking spaces may be substituted for short-term on a two for one basis up to a maximum of fifty (50) percent of the required short-term spaces.
- Long-term bicycle parking for multiple use developments and shopping centers may be reduced up to 10 percent.
- Long-term bicycle parking must have controlled access. Alternative examples to Section 3.3.9.4.B of the LUC include, but are not limited to:
 - Valet Bike Parking: The valet parking system works much like a coat check: the bicyclist gives their bicycle to the attendant, who tags the bicycle with a number and gives the bicyclist a claim stub. When the bicyclist returns to get their bicycle, they present the claim stub and the attendant retrieves their bicycle for them. Locks are not needed.
 - Within view of an attendant or security guard (maximum 100 feet).
 - In an area that is monitored by a security camera.
 - In an area that is visible from employee work areas.
- Only 50 percent of required long-term bicycle parking must be covered.



3. Parking Area Access Lanes

Parking Area Access Lanes (PAALs) shall be a minimum of twenty (24) feet in width for a two-way PAAL. A 20-foot PAAL may be used as a one-way access aisle.

4. Paving Materials

Vehicle use areas shall be constructed utilizing materials and construction techniques in accordance with the recommendations of the geotechnical engineer and concurrence from City of Tucson staff.

5. Handicapped Parking

Handicapped parking will be provided in accordance with ADA requirements from the 2006 IDC, Chapter 11 and ICC/ANSI 117.1, 2003 Edition.

6. Major Streets and Routes Plan

Valencia Road and the future Spine Roadway running from the Drexel Road alignment, south through the project site to Valencia Road are considered arterial roadways within the City of Tucson Major Streets and Routes Plan (MS&R) Plan. The future ROW for Valencia Road is 200 feet in the vicinity of the site. The proposed Spine Roadway has a future right-of-way dedication of 100 feet. Required capacity improvements are subject to development impact offsets and credits.

7. Loading Zones

The PAD will comply with the Loading Requirements of Division 3 of Article 3 of the LUC with the following exceptions:

- All loading areas may be provided at off-street parking spaces and at designated on-street locations posted for such use, provided that the loading space is located within 250 feet of the use it serves and shall be the same size as the parking spaces.

8. Solid Waste & Recycling Material Disposal & Storage

All required Solid Waste and Recycle materials collection and storage shall be located and screened, to allow for safe access and maneuverability within the development. Enclosures for the storage of containers shall be designed in accordance with Environmental services development standards No. 6-01.0, for solid waste and recycle disposal.

9. Accessibility

a. Governing Accessibility Codes

All development within the PAD will comply with accessibility requirements by adhering to the:

- City of Tucson, Department of Transportation development standards for all accessibility requirements within public right-of-way
- 2006 IBC, Chapter 11 and ICC/ANSI 1117.1, 2003 Edition.



- If any part of this project is to be funded by public monies, that portion of the project shall identify if ADAAS 2010 is to be the accessible standard required by the funding source.

b. Accessible Routes for New Improvements

All improvements will comply with accessible code standards for curb ramps, sidewalks, detectable warnings and marked crosswalks.

c. Pedestrian Accessibility & Van Accessibility

All ADA compliant pedestrian accessibility must be provided to public right-of-ways throughout the site. Accessible spaces and "Van Accessible" spaces will connect to the accessible routes as required by the 2006 IDC, Chapter 11 and ICC/ANSI 117.1, 2003 Edition. All sidewalks, detectable warnings and curb ramps will comply with accessibility requirements as required.

10. Wastewater

The owner/developer shall obtain written documentation from the Pima County Regional Wastewater Reclamation District (PCRWRD) that treatment and conveyance capacity is available for any new development within the rezoning area, no more than 90 days before submitting any tentative plat, development plan, sewer improvement plan or request for building permit for review. Should treatment and/or conveyance capacity not be available at that time, the owner/developer shall have the option of funding, designing and constructing the necessary improvements to Pima County's public sewerage system at his or her sole expense or cooperatively with other affected parties. All such improvements shall be designed and constructed as directed by the PCRWRD.

D. Landscape Borders and Screening

The proposed landscape program includes the creation of new landscape borders and screening between uses. As shown on Exhibit III.D: Landscape Border Plan (page 52) 5-foot screening walls will be installed in conjunction with minimum 10-foot wide landscape borders where there is existing land uses adjacent to the site:

- Parcel west of I-10: along a portion of the west boundary where adjacent to residential uses or zones.
- Parcel east of Valencia: along the eastern and southern boundaries where adjacent to school and residential uses or zones.

The landscape program shall also adhere to the following requirements:

- Landscape Border and Screening requirement for all Large Scale Retail Development are found in Section III.B.1.a on pages 42-43.
- Loading and delivery docks, outdoor storage areas, garbage and recycling areas and other similar exterior improvements facing residential uses or residentially zoned properties outside of the PAD boundaries, adjacent public roadways, and the Julian Wash Greenway will be screened (unless already substantially screened by buildings or other landscaping) with walls a minimum of 6-feet high.



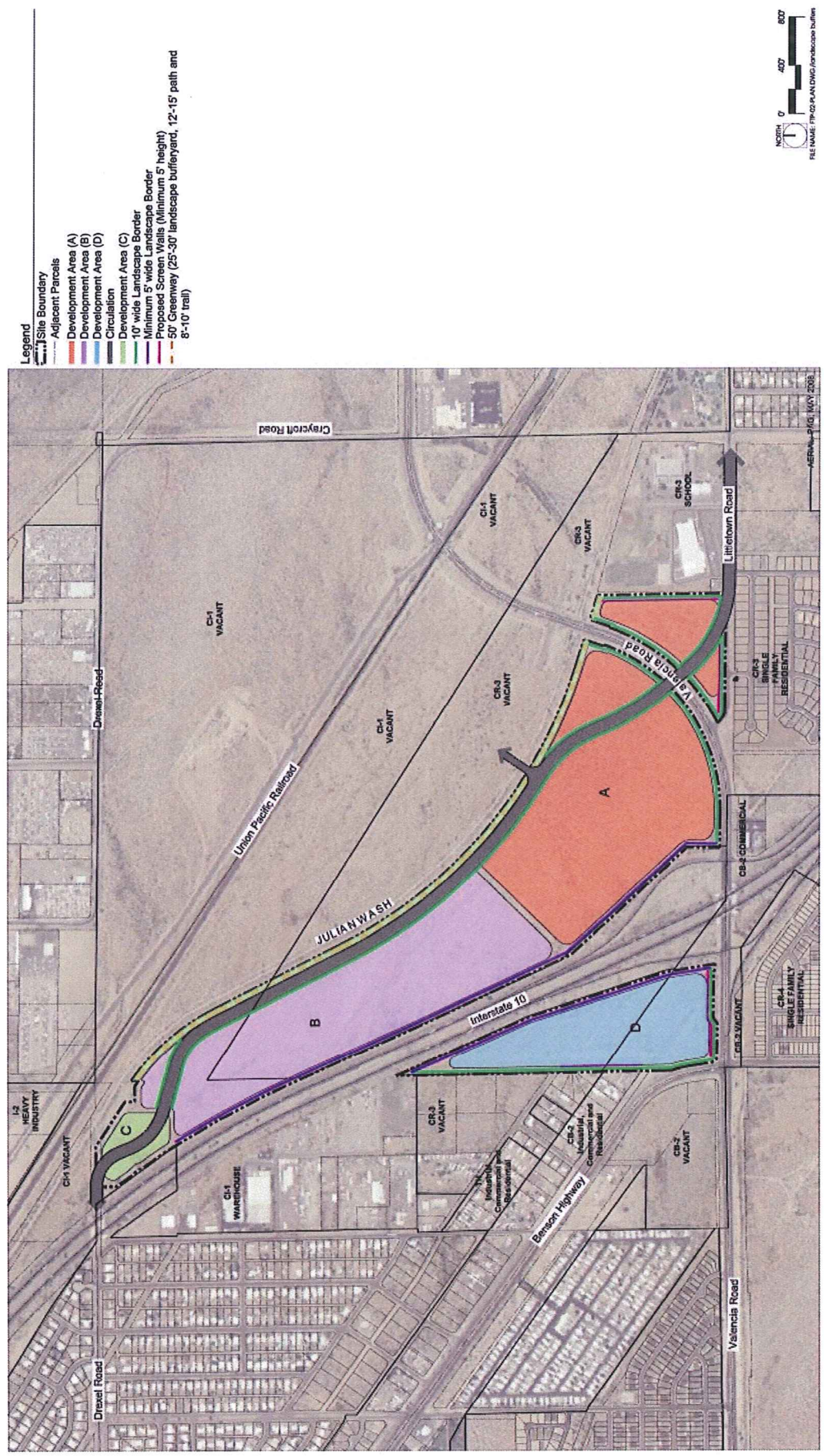
- Street landscape borders and screening of parking lots along Valencia Road and Spine Road will be accomplished with low screen walls (consisting of a wall/vegetative hedges/combination of both) and 10-foot wide landscape border in accordance the City of Tucson Land Use Code.
- Screening of parking lots along Julian Wash Trail shall have a maximum 30-inch screen consisting or wall, vegetative hedge, or a combination of both.
- The site is subject to City of Tucson Commercial Rainwater Harvesting requirements.
- A Native Plant Preservation Plan (NPPO) shall be prepared on a Development Area basis. A registered landscape architect shall determine the viability of conducting a NPPO for each block. In the event there are native plants that can be transplanted or preserved in place, the vegetation existing on the site shall be inventoried and a plan shall be submitted by a registered landscape architect to the City of Tucson in accordance with the provisions set forth in the City of Tucson Land Use Code.
- All landscape frontages adjacent to Interstate 10 will be in conformance with the City of Tucson Land Use Code.
- Non-residential uses shall consist of a minimum of 10-foot wide landscape buffer and 5-foot decorative masonry wall unless developed adjacent to non-residential uses.
- Where a single parking row occurs, one tree per every four parking spaces will be provided.
- Where a double parking row occurs, one tree per every eight parking spaces will be provided.
- All landscape borders proposed within public rights-of-way will require approval from the Tucson Department of Transportation (TDOT).
- Low-water-use plants that reflect and enhance the image of Sonoran Desert landscape will be utilized.
- Provide shade along pedestrian circulation and trail node areas.
- Provide shade throughout the development utilizing trees and/or building overhangs/arcades
- Irrigate landscape areas using an underground drip irrigation system.
- Coordinate above- and below-ground utilities in landscape plans to avoid conflicts at the time of installation.
- Minimize the removal of mature trees, cacti, and other native vegetation during site design. If in-place preservation is not feasible, viable mature vegetation should be salvaged and replanted on-site.
- The following landscaping standards should enhance the development through the use of one or more of the following delineation methods:
 - Access to public areas of interest such as parks, plazas and trail nodes will be enhanced through the use of additional or colorful plantings, monumentation or artwork.
 - Include outdoor public spaces with special amenities that encourage use, such as benches, overhead shade, sitting areas, outdoor fitness equipment, picnic tables, play equipment, mail boxes and bicycle facilities.
 - Incorporate seating, landscape, and gathering areas that contribute to the outdoor use and the open space enjoyment of the area in spaces between buildings in a development.



- Use plant selection and placement to reduce the heat island effect.
- Use bold accents and colorful plant species to liven up the street scene and intersection.



Exhibit III.D: Landscape Border Plan



E. Post-Development Hydrology

1. Response to Hydrologic Characteristics

The Land Use Concept (page 41) responds to the hydrologic characters identified in the Site Inventory. The design is intended to accept offsite flows and to discharge onsite flows at their current locations and with their current characteristics. There will be no increase of runoff leaving the site.

2. Encroachment

The Julian Wash, a WASH Ordinance Wash, and required 50-foot setback will remain undisturbed with the exception of a 50-foot Julian Wash Greenway as required by City of Tucson Parks and Recreation. Public process requirements for the development of the Julian Wash Greenway will take place during the PAD process and thus, public process procedures during the multi-use trail development will not be required. Engineered channels will be provided to route all on-site and off-site runoff through the site in a manner that will not have any adverse impacts on adjacent properties. Where necessary, adequate bank protection will be used to reduce erosion hazard setbacks. Detention and retention basins will be used to mitigate any increases in the developed flows.

3. Post-Development Water Discharge

The majority of the site is located within the Julian Wash Watershed, which is a Balanced Basin, and as required by City of Tucson standards; post-development runoff exiting the site will not be increased. Watershed 3 is in the Earp Wash Watershed, which is a Critical Basin. The post development flow for this watershed will be reduced by at least 15 percent. The estimated onsite post-development runoff has been mapped. (See *Exhibit III.E: Post-Development Hydrology Concept*, page 55.) The cumulative post-development discharge at Concentration Point 1 is 318 cfs, 1240 cfs at Concentration Point 2, 55 cfs at Concentration Point 3 and 93 cfs at Concentration Point 4 and 230 cfs for Concentration Point 5. As previously discussed, the post-development runoff will be mitigated by detention and retention to ensure that the runoff leaving the site is equal to or less than existing conditions.

4. Engineering and Design Features

Due to the conceptual nature of the PAD, no potential engineering features can be identified at this time. Detention and retention will be provided on each portion of the site per City of Tucson standards.

5. Basin Management Policies

The PAD's Land Use Concept (page 41) conforms to all City of Tucson policies applicable to basin management. All requirements within DS Sec.9-06.2.0 for regulated

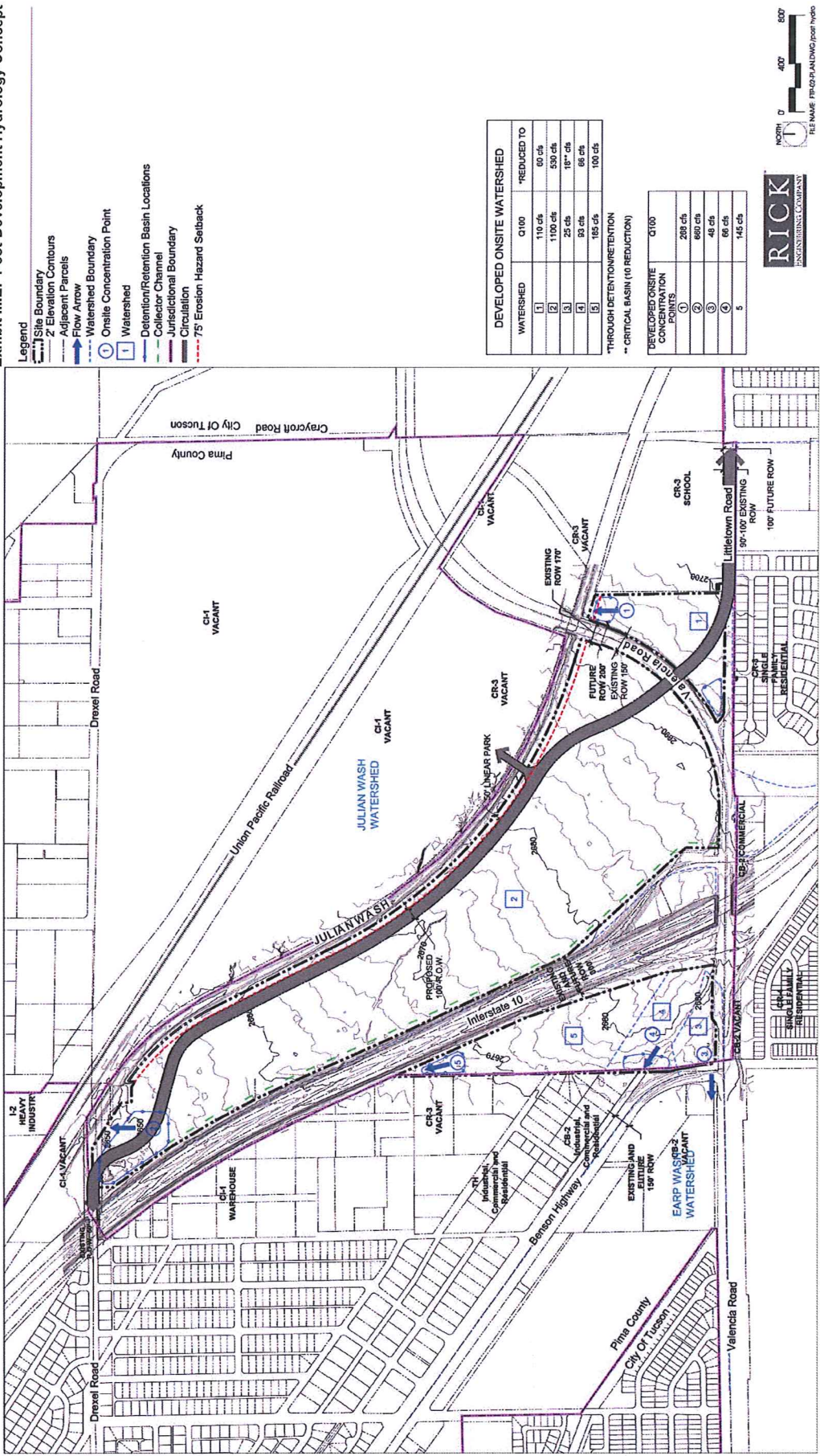


100-year floodplain apply to the PAD development and future plans must provide a Drainage Report plus any applicable Environmental Resource Report for proposed disturbances within a regulated 100-year floodplain. It is anticipated that the Julian Wash completely contains the 100-year regulatory base flood and there will be no construction of buildings within the regulated erosion setback of the Julian Wash; however, if there is any bank protection in a regulated riparian habitat area or 100-year floodplain, it will need to be justified in a Drainage Report showing that the improvement is necessary for development.

In addition, the erosion hazard setback area from any regulatory watercourse must be determined by a separate Drainage Report specific to the watercourse in question at time of development plan submittal. All requirements within DS Sec.10-02.7.6.1 for determining the erosion hazard setback from a regulated watercourse will be required at time of submittal



Exhibit III.E: Post-Development Hydrology Concept



F. Biological Resources

Per a preliminary evaluation of the on-site vegetation, the Pima County mapped Xeroriparian Type D habitat, *as shown on Exhibit II.1: Composite Map on page 38*, is considered Low Habitat that has no connectivity between higher habitat classes in the area. The existing vegetation does not contain low-volume, high-value habitat. The areas adjacent to the site contain only Xeroriparian Type D habitat. Interstate 10, Valencia Road, and existing development serve as boundaries to the Xeroriparian Type D habitat on site, and the site does not provide connections to higher value habitat classes.

In addition, there are no areas Tobosa, Black Grama, or other grassland habitats that would be considered low-volume, high-value habitat located on the site. The majority is covered with large numbers of Creosote bush with smaller numbers of Cholla, Prickly Pear, Barrel cacti, and some Mesquite trees. See Section II.E for more information on existing vegetation.

The Julian Wash is regulated by the WASH Ordinance and, to the extent that flows of 100 cfs or greater exist within the 100-year floodplain, the requirements of Development Standard Sec.9-06.2.0 apply to the PAD development, with the following exceptions:

- In the event that the 100-year floodplain coincides with Pima County Xeroriparian Habitat D, any required riparian mitigation will be located within the 50-foot Julian Wash Greenway per the approval of COT Parks and Recreation Department. Determination of mitigation requirements shall be based on a modified Environmental Resource Report process (to be agreed upon by City of Tucson staff and the developer). The modified process will at a minimum include on-site evaluation (quantity and quality) of the areas of Pima County riparian habitat coinciding with 100-year Floodplain that are proposed for development.
- It is anticipated that the Julian Wash completely contains the 100-year regulatory base flood and there will be no construction of buildings within the regulated erosion setback of the Julian Wash or the required 50-foot setback adjacent to a WASH Ordinance Wash with the exception of a 50-foot Julian Wash Greenway.
- All public process requirements for the WASH Ordinance and the Julian Wash Greenway will be deemed to have taken place concurrently with during the PAD application, and no separate application, public notice or public process will be required..

G. Cultural Resources

A review of Pima County records revealed that the entire PAD site has been previously surveyed for cultural resources in 1987 and 2000 (1987-219.ASM, 2000-116.ASM, and 2000-189.ASM). No archaeological or historic sites have been recorded within the property. The undeveloped property is bounded on the northeast by a channelized section of Julian Wash, but is designated to have Low Archaeological Sensitivity as defined in the Sonoran Desert Conservation Plan (SDCP). Historic or prehistoric features or artifacts discovered during future ground disturbing activities should be reported to the City of Tucson Historic Preservation Officer. Pursuant to A.R.S. 41-865 the discovery of human remains and associated objects found on private lands in Arizona must be reported to the Director of Arizona State Museum.



Historic or prehistoric features or artifacts discovered during future ground disturbing activities should be reported to the City of Tucson Historic Preservation Officer. Pursuant to A.R.S. 41-865 the discovery of human remains and associated objects found on private lands in Arizona must be reported to the Director of Arizona State Museum.

H. Transportation Infrastructure

1. Proposed Rights-of-Way

All proposed streets rights-of-way within the boundaries of the Valencia Crossing PAD will be in conformance with City of Tucson street standards.

2. Traffic Impact Analysis

All transportation planning for Valencia Crossing is based on a traffic study that has been submitted under separate cover. This traffic study provides an overview of traffic operations and recommended transportation improvements based upon development in the project vicinity and the current capacity of the surrounding roadways. A more detailed traffic impact study will be required during the development plan stage once the land uses have been refined and determined whether the intensities are compatible. Some of the conclusions and recommendations of the current traffic study indicate:

Publicly-funded roadway improvements include the widening of Valencia Road to six lanes between Alvernon Way and Kolb Road between years 2012 and 2016 and upgrades to the Valencia and I-10 interchange.

Based on future (2015) evaluation of the daily traffic volumes, all roadways are anticipated to operate a level or service of C or better. In fact, most roadways will remain at level of service A, which provides the best traffic operations to the public. This assumes a widened 6-lane Valencia Road.

3. Proposed Circulation

The project area will have access points from Valencia Road, Littletown Road, Drexel Road and Benson Highway. As shown in *Exhibit III.G.3: Proposed Vehicular, Bicycle & Pedestrian Circulation* (page 59), the main access to the site (spine roadway) will be provided by a realigned Littletown Road through the project site. This realignment will allow for a traffic signal at the new Littletown Road and Valencia Road intersection providing for a greater separation between Interstate 10 and the main access to the site. There will be no accesses to property in the ADOT/I-10 access control.

Internal site circulation for the proposed development will be provided by a new north-south spine roadway connecting from Valencia Road to Drexel Road. The Spine Road will be an arterial roadway with a proposed 100-foot right-of-way. The constructed cross-section could be reduced near the north-end of the subject property where land uses are not anticipated to be as intense as the commercial portion near the south boundary; however, the ultimate right-of-way of 100 feet would need to be dedicated through the



entire project length. Any reduction in the Spine Road cross section will need to be supported by a Traffic Impact Analysis. Street rights-of-way will be dedicated to City of Tucson via block plat, and streets will be constructed in accordance with City of Tucson standards.

In addition, two unsignalized median openings will provide access from Valencia Road to the proposed commercial areas north and south of Valencia Road and shall be spaced in accordance with COT Access Management Guidelines. Final locations for these unsignalized access points are undetermined until further development plans are completed. Secondary and emergency access will be provided via Drexel Road at the north. Access to the parcel west of Interstate 10 will be from Benson Highway; no access off Valencia Road will be permitted to this parcel. Future construction of the proposed access across the Julian Wash will be determined by adjacent property owner.

TDOT will require appropriate improvements to the abutting roadways as determined by a Traffic Impact Analysis to be prepared by the owner/developer and submitted to TDOT for review, comment and approval.

Access to the adjacent parcel to the east across Julian Wash is to be provided by the adjacent property owner. Phasing of this improvement is undetermined.

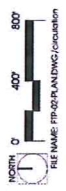
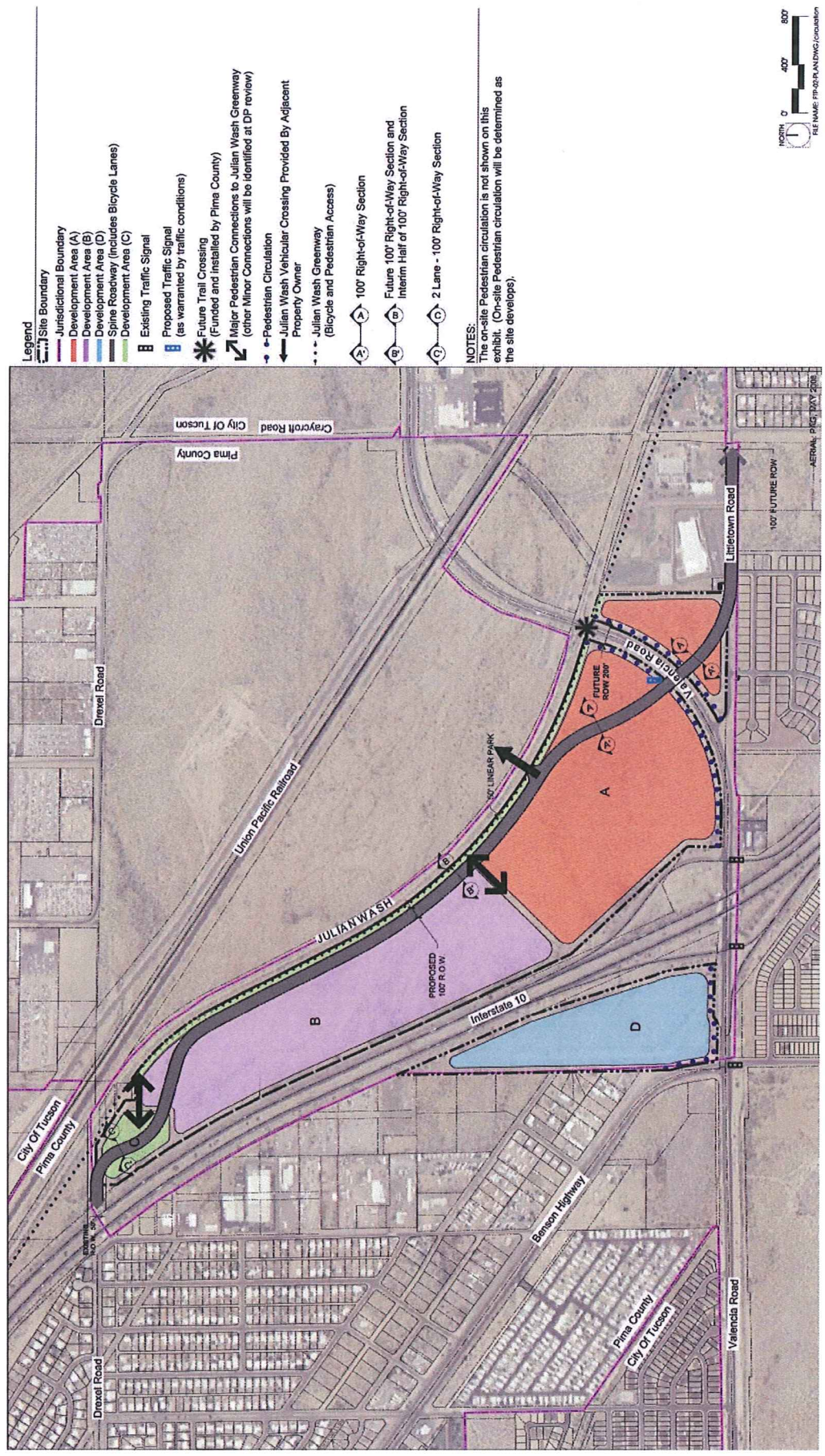
4. Spine Roadway

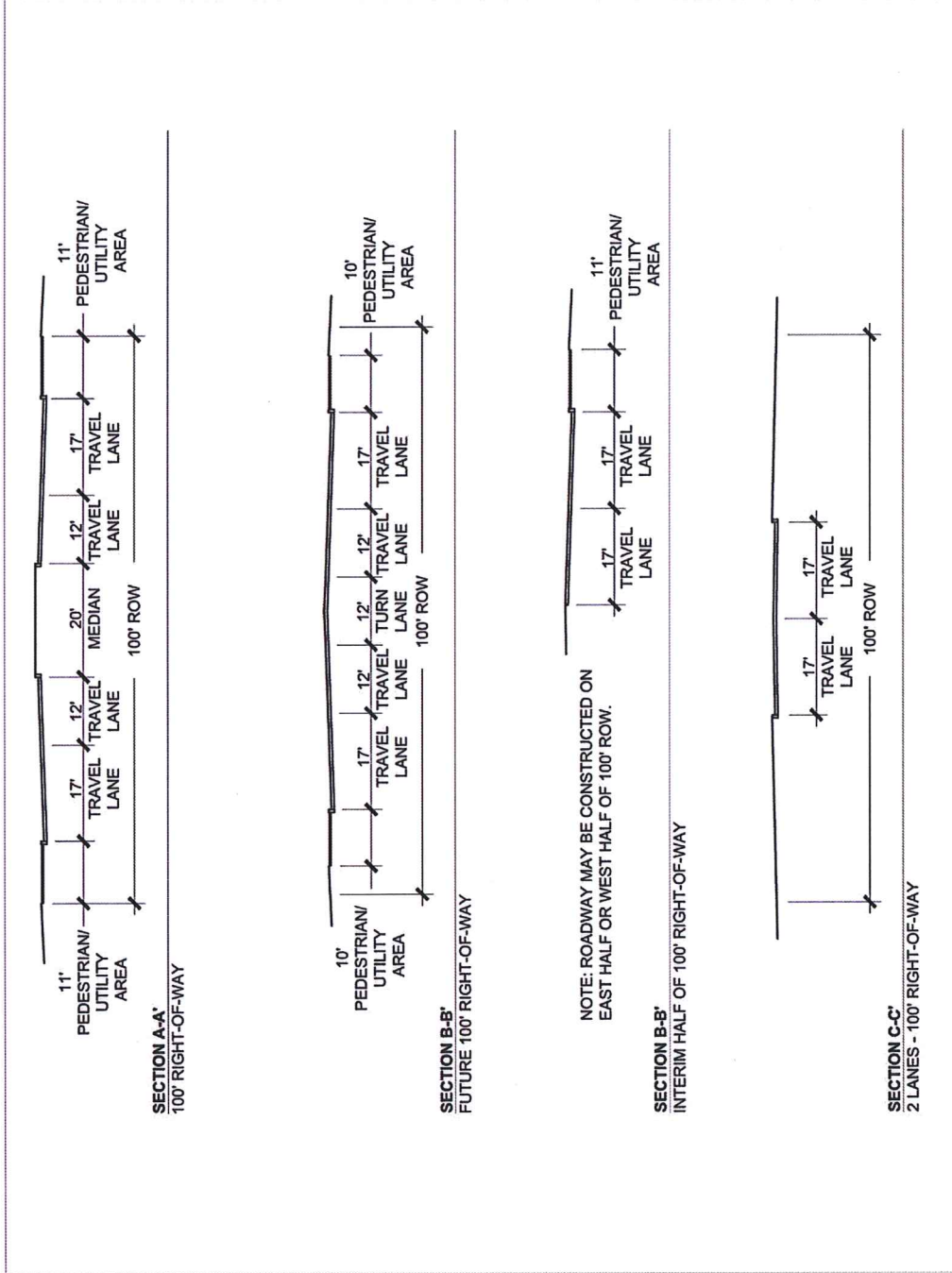
Three separate cross-sections are proposed for the spine roadway as shown on page 60 and referenced on *Exhibit III.G.4*. Cross Section A-A is designed based on the example MS&R “100-foot right-of-way” and provides for two lanes in each direction with a raised median. This will be constructed as part of the initial phases and will provide access control through the southern-most portion of the development where traffic volumes will be the highest. Cross-Section B-B is based upon the example MS&R “90-foot right-of-way” and provides for two lanes in each direction with a center left-turn lane. This will provide flexibility in access and circulation through the middle of the project. An interim phasing of this cross-section is proposed for the initial connection of Drexel Road to Valencia Road and is based upon utilizing approximately half of ultimate cross-section. The specifics of the interim cross-section, such as cross-slope and drainage, will be determined by future engineering studies. Cross-section C-C on the north end of the project will provide one lane in each direction to match the existing Drexel Road configuration and the available bridge width under I-10.

As previously noted, the full 100-foot ROW will be dedicated throughout the project regardless of the pavement width.



Exhibit III.G.3: Proposed Vehicular, Bicycle & Pedestrian Circulation





5. Valencia Road/Littletown Road Improvements

The Valencia Road widening is a RTA/City of Tucson funded improvement proposed for widening from four to six lanes from 900' east of Alvernon Way to 750' east of Wilmot Road. The underpass at Interstate 10 (I-10) and the Union Pacific Railroad (UPRR) Bridge will also be widened. At the west end, the project will tie into the Alvernon Way/Valencia Road intersection improvement project, which is currently advertised for construction. The east end of the project will tie into the Valencia Road, Wilmot Road to Kolb Road project, which is also under design. In addition, the Capital Improvement Plan intends to provide for a pedestrian path underneath the then newly widened bridge.

As part of the Valencia Road widening, Littletown Road will be realigned with a 100-foot right-of-way through the subject property, south of Valencia Road. The conceptual alignment is shown on *Exhibit III.G.3 on page 59*. The Master Association will be responsible for right-of-way dedication as part of the PAD approval process.

6. Impact To ADOT Interchange

The off-site impact to the Arizona Department of Transportation (ADOT) interchange can be mitigated by additional turn lanes at the ramp intersections. Major reconstruction of this facility is not anticipated. Arizona Department of Transportation is currently conducting a location study for Interstate 10 from Interstate 19 to State Route 90 (Benson). This study will identify locations for interchanges including the potential extension of the Barraza Aviation extension. The study will likely finalize recommendations near the end of 2011. Following the results of the location study, a design concept evaluation will commence. This stage, extending 2 years, will evaluate the interchange configurations and the extent of potential right-of-way needs and construction costs.

7. Barraza Aviation Alignment

The Barraza Aviation Parkway currently extends from Downtown Tucson to Golf Links Road. The Regional Transportation Authority (RTA) has allocated funding to study the potential extension of this corridor to the east and acquire necessary right-of-way. Numerous alternatives have been identified for the extension alignment ranging from the existing Alvernon Way to Wilmot Road. The ongoing ADOT I-10 Location Study will evaluate the alternatives and recommend a preferred alignment.

8. Gateway Route

Valencia Road, located adjacent to the subject property is considered a Gateway Route; and therefore, is subject to the regulations of Section 2.8.4: Gateway Corridor Zone of the City of Tucson Land Use Code with the following exception: The public process for deviations from gateway standards shall be combined with the PAD public process zoning examiner legislative procedures.



I. Recreation, Bicycle & Pedestrian Circulation

As part of the platting and development review process, a proposed recreation, bicycle and pedestrian plan will be submitted that describes the proposed trail connections and any other recreational features to be provided as part of the PAD development in conformance with the City's guidelines. The Valencia Crossing Master Association shall determine the ultimate location of all recreational, bicycle and pedestrian features..

1. Pedestrian Connections

Pedestrian access will be required throughout the development. All public rights-of-way will be constructed to City of Tucson Street Standards, and where bicycle lanes are required, all on-street bicycle lanes will be built to City of Tucson standards. In addition, the following will be incorporated into the PAD:

- A continuous network of on-site pedestrian walkways will be provided to allow for direct access and connections to and between the following:
 - The primary entrance or entrances to each commercial building on the site;
 - Any sidewalks or walkways on adjacent properties that extend to the boundaries shared with the commercial development;
 - Public sidewalks along the perimeter streets adjacent to the commercial development;
 - Adjacent land uses and development including, but not limited to, adjacent residential developments and retail shopping centers; and
 - Shading along pedestrian paths will be provided.
- At each point that a designated on-site pedestrian walkway crosses a parking lot, street or driveway, the walkway will be clearly visible to pedestrians and motorists through the use of one or more of the following delineation methods:
 - A change in paving material, paving height or paving color;
 - Decorative bollards;
 - A painted crosswalk;
 - Signage; or
 - A safely delineated median walkway buffered by landscaping.

2. Julian Wash Greenway

Property for a 50-foot wide linear trail and associated landscaping along the eastern edge of the site adjacent to the Julian Wash will be dedicated and transferred to Pima County. See Exhibit III G. 3 on page 59. The trail is to be constructed in accordance with Pima Regional Trail System Master Plan standards as part of the Pima County Bond program. The trail will contain landscape buffers, a 12- to 15-foot multi-use trail and an 8- to 10-foot decomposed granite path. Construction of the trail is presently planned to commence in late 2012.

. See Section III.I: Phasing for more information on Julian Wash Greenway Phasing Plan. The following Julian Wash Multi-Use Trail standards apply:

- The Julian Wash Multi-Use Trail will have pedestrian/bicycle connections to Development Area A, B and C within the Valencia Crossing PAD and will be addressed at time of Development Plan review. See *Exhibit III.G.3: Proposed Circulation, Bicycle & Pedestrian Circulation* (page 59). At each connection point that a designated on-site



pedestrian walkway crosses the spine roadway, the pedestrian walkway will be clearly visible to pedestrians and motorists through the use of one or more of the following delineation methods:

- A change in paving material, paving height or paving color;
 - Decorative bollards;
 - A painted crosswalk;
 - Signage; or
 - A safely delineated median walkway buffered by landscaping;
- Vehicular and pedestrian access across the Julian Wash to the property to the east will be addressed by the adjacent property owners;
 - A pedestrian path crossing at Valencia Road will be designed and installed by Pima County Department of Transportation in association with the Regional Transportation Authority Improvements along Valencia Road; and

3. Development Area C Drainage Area

It has not been determined as to what type of features will be incorporated into Development Area C at this time; however, the following uses are permitted, including, but not limited to, communications facilities, drainage facilities, neighborhood recreation, open space, public utilities and trails. At the time of drainage basin installation, the drainage basin will be designed to accommodate any potential passive or active recreation uses in accordance with Chapter 4 of the Stormwater Retention/Detention Manual.



J. Infrastructure Phasing

The general phasing of the project is expected to start with Development Area A portion of the PAD area, along with the infrastructure required to serve the development with the expectation that Development Areas B, C and D will follow in accordance with market demands. The phasing plan is subject to change; however all necessary infrastructure will be provided to accommodate the phasing of the development. The following is a conceptual phasing plan for PAD infrastructure improvements separated into three stages: initial improvements, mid-term improvements and full build-out improvements. Off-site improvements will not be eligible for Impact Fee Credits until the Spine Road has demonstrated connectivity from Drexel Road to Valencia Road. See Appendix A: Article 4.6.

1. Pima County/Regional Transportation Authority (RTA) Transportation Improvements

- Valencia Road widening plans and construction documents from Alvernon Way to Wilmot Road are underway. The roadway will be widened from 4 to 6 lanes with bike lanes and sidewalks in the early period of the Regional Transportation Plan. The early period ranges from 2011-2020.
- Littletown Road realignment (south of Valencia Road) and intersection improvements- Plans and construction documents for the Littletown Road realignment through the subject property south of Valencia Road are underway. Improvements will be made in conjunction with the Valencia Road widening in the early period from 2011-2020.
- Traffic signal at Valencia Road and the future Littletown alignment- Plans and construction documents for the traffic signal at Valencia and the Littletown Road realignment are underway. Improvements will be made in conjunction with the Valencia Road widening in the early period from 2011-2020.
- The Julian Wash Greenway and landscaping improvements along the Julian Wash Greenway will be constructed by Pima County in late 2012
- Julian Wash Pedestrian Crossing at Valencia Road- Plans and construction documents for the pedestrian crossing at Valencia are underway. Improvements will be made in conjunction with the Valencia Road widening in the early period from 2011-2020.

2. Initial PAD improvements

- If the PAD Development commences prior to the Valencia Road RTA improvements, the developer of the area in question will work with Pima County Transportation and the Regional Transportation Authority to construct the required interim improvements as determined necessary by a Traffic Impact Analysis for the PAD to function.
- Spine Roadway (north of Valencia)- Before any additional certificates of occupancy are issued or any final inspections are approved for any use on Development Area A north of Valencia Road; the spine road extending north from Valencia Road will be constructed in segments as needed to serve the relevant portions of Development Area A, north of Valencia Road. Off-site improvements will not be eligible for Impact Fee Credits until the Spine Road has demonstrated connectivity from Drexel Road to Valencia Road. See Appendix A: Article 4.6.



- Pedestrian connections- Pedestrian connections from the Julian Wash Greenway to Development Area A shall be constructed as part of the linear park improvements for Development Area A. A pedestrian crossing at Valencia Road is a part of the Pima County's plan for Valencia Road improvements and will be constructed by others between 2011 and 2020. Any access across the Julian Wash Greenway from the subject property shall be constructed by others; and phasing for any such access is undetermined. Planning & Development Services Department, City of Tucson and TDOT will be consulted for approval regarding the design and location of any major pedestrian connection (as shown in Exhibit II.G.3) between Development Area A and the Julian Wash Greenway.
- Detention- Detention and Retention will be provided in accordance with the master hydrology/drainage report.
- The single existing billboard located on Development Area D shall be removed within 370 days following submittal of the first development plan within the PAD boundaries.

3. Mid-term improvements

- Spine Roadway (north of Valencia)- Before any certificates of occupancy are issued or any final inspections are approved for any use on Development Area B or C, the spine road extending north from Valencia Road into Development Areas B and C will be constructed in segments to connect to Drexel Road.
- Pedestrian connections- A pedestrian connection from the Julian Wash Greenway at the north end of Development Area B and C shall be constructed as part construction for Development Area B and C. Planning & Development Services Department, City of Tucson and TDOT will be consulted for approval regarding the design and location of any major pedestrian connection (as shown in Exhibit II.G.3) between Development Area B & C and the Julian Wash Greenway.
- Detention/Retention- Detention and Retention will be provided in accordance with the master hydrology/drainage report.

4. Full build-out improvements

- Spine Roadway- The connection of the Spine Road from Drexel to Valencia will need to be completed prior to the issuance of Certificate of Occupancies for the final phase of this PAD development.
- Transportation infrastructure- Before any certificates of occupancy are issued or any final inspections are approved for any use on Development Area D; adequate access to Development area D must be provided as determined by a Traffic Impact Analysis. Off-site improvements are not anticipated along Benson Highway.
- /Retention/Detention and Retention will be provided in accordance with the master hydrology/drainage report

K. Schools

It is anticipated that this project will generate approximately 384 high-density residential units if the Mixed-Use Business Park areas develops with approximately 16 acres of residential. However, that number may decrease if the market demands employment or commercial uses over residential uses.



Preliminary discussions with Sunnyside School District officials have indicated that there is capacity within these schools sufficient for the projected number of children within Valencia Crossing. In addition, a separate agreement allowing for a voluntary contribution for SUSD Schools will be sought if and when any residential is constructed on the property.

L. Water Conservation

- All developments within the PAD will submit a water harvesting plan that will be included with the development plan submittal in compliance with Development Standards 10-03 illustrating 50 percent of estimated landscape water budget is met by water harvesting techniques.
- Use water-efficient native and drought tolerant plants for landscape and limit turf areas to community recreation areas (public or private): All turf areas limited to 30% of community recreation areas, or if there is more than 30% turf, explanation of water conservation measures is required to limit potable water source to irrigate turf areas.
- Reduce water consumption by using water conserving plumbing fixtures.
- Minimize impervious surfaces in areas of high pedestrian oriented areas (i.e., pathways, plazas, courtyards, bicycle parking, pedestrian pathways in parking lots), area adjacent to planting areas (i.e, tree wells in vehicular parking areas)to lessen the heat island effect and reduce stormwater runoff by using paving alternatives and decreasing the overall amount of pavement throughout the development.
- Use water harvesting and provide opportunities for grey water uses. Employ water harvesting techniques, such as directing roadway rainfall runoff into adjacent landscaped areas and grading lots to direct rainfall runoff from roof and/or paved surfaces into landscaped areas by way of swales and micro-basins.
- Install high efficiency and evapotranspiration (ET)-based drip irrigation systems for commercial, municipal, and street landscape.

M. Valencia Crossing Development & Design Standards

Prior to development of any area within Valencia Crossing, detailed development and design standards will be submitted as an extension of this document. These standards will establish common theme and design elements that will be used throughout the project area. These standards will cover unifying site design elements, including streetscape design, signage, materials, colors and architectural styles and will be reviewed and approved by the design review committee



N. Design Review

The Master Association shall establish a Design Review Committee (DRC), prior to submittal of any development plan or tentative plat, which will be responsible for reviewing and approving all details of project design through a self-certification process. The Design Review committee shall consist of an architect, a landscape architect and a member of the master association. The DRC shall review and approve/disapprove the architectural design of all development within the PAD District to ensure it is consistent with the Valencia Crossing Design Guidelines. A copy of the self certification will be provided to the City of Tucson at the time of development plan/subdivision plat submittal advising whether the design conforms to the standards established by the DRC.

O. Valencia Crossing Master Association

The implementation of the Valencia Crossing PAD is the responsibility of the Builder, the Valencia Crossing Master Association and the City of Tucson. The Valencia Crossing Master Association, or their successors and assigns, is responsible for providing the basic infrastructure needs including roads, sewer and water in a timely and efficient manner. The Valencia Crossing Master Association is responsible for recording the Master Conditions Covenants and Restrictions (CC&R).

The Builder, residential or commercial, is the purchaser of a development area, or portions of a development area, which may or may not require additional infrastructure improvements, such as utility extensions or local streets. The Builder is responsible for all construction and lot improvements not made by the Master Association within their area(s) of ownership. Should the Builder purchase a development area without the infrastructure improvements generally provided by the Master Association, the Builder becomes the responsible party for the construction or installation of those improvements.

P. Interpretations and Amendments

1. Interpretation

The regulations and guidelines provided within this PAD supersede existing regulations within the City of Tucson Land Use Code and Development Standards. If an issue arises regarding definitions, conditions, standards and/or situations not addressed in this PAD, those in the LUC, Development Standards or other COT regulations shall prevail, as interpreted by the COT Zoning Administrator.

2. Amendments

Amendments to the Valencia Crossing PAD may be necessary over time in order to respond to the changing market demands, or financial conditions, or to respond to the unanticipated needs of new users. Non-substantial changes to the PAD shall be approved pursuant to LUC Section 2.6.3.11.B.5 including the following:

- Minor changes to the PAD boundary as a result of Littleton Road realignment.



Substantial changes (as defined in LUC Section 2.6.3.11.B.3), are subject to the amendment process outlined in LUC Section 2.6.3.11.B.4.

3. Definitions

- **Builder:** The Builder is the purchaser of a parcel within the project who proposes improvements to the land and submits plans in relation to such improvement.
- **Business Park:** A business park is an area of land in which several office buildings are grouped together that specialize in commercial enterprise.
- **Design Review Committee:** A committee whose function is to review and approve all details of project design to ensure the project is developed in accordance with the approved development standards.
- **Development Areas:** Development Areas "A through D" as shown on Exhibit III.A: Land Use Concept Plan.
- **Drainage Facilities:** Stormwater Drainage Facilities: Drainage facilities that include but not limited to engineered swales and channels, water harvesting areas, detention/retention ponds, and underground pipe systems. The purpose of the storm drainage facilities is to provide for the protection of abutting and off-site properties that would be adversely affected by any increase in runoff attributed to the development.
- **Incubator Space:** seeks to nurture young start-up companies during their early years. The incubators offer inexpensive space, shared services and management help. An example might be a biomedical research incubator near a medical school or an incubator that attracts software developers located near a university with expertise in that area.
- **Live/Work Unit:** An industrial, office or commercial land use activity carried out for financial gain by a resident and/or artisan, on the resident's property. This type of uses may occur within industrial and/or commercial areas.
- **Long-term Bicycle Parking:** the purpose of long-term bicycle parking is to provide employees, students, residents, commuters and other who generally stay at a site for several hours, a secure and weather-protected place to park bicycles.
- **Master Association:** The Valencia Crossing Master Association is a non-profit entity that governs all other sub-associations within the community. The Association establishes the Design Review Committee (DRC) that reviews all plans for proposed construction within the PAD area. It is also responsible for the enforcement of the C, C, & R's, and maintenance and management of all



shared private common areas that are not dedicated to the City of Tucson or maintained by a subdivision homeowners' association.

- **Mixed Use Development:** A tract of land, building, or structure developed for two or more different uses such as, but not limited to, residential, office, retail, public, or entertainment. The mix of uses may occur either on the same tract of land, but compartmentalized into separate buildings, or, located within the same building (e.g. retail on the first floor and office or retail on the floors above the retail).
- **Open Space:** is an area of land that remains essentially unimproved and not occupied by structures or man-made impervious surfaces, except public non-motorized recreation trails, that is set aside, dedicated, or reserved in perpetuity for public or private enjoyment.
- **Outdoor Storage :** The location of any goods, wares, merchandise, commodities, debris or any other item outside of a completely enclosed building for a continuous period longer than 24 hours.
- **Short-term Bicycle Parking:** The purpose of short-term bicycle parking is to encourage shoppers, customers, messengers, and other visitors to use bicycles by providing a convenient and readily accessible place to park bicycles.
- **Trails:** Unpaved or paved trails or paths permitted for use only by pedestrians, nonmotorized bicycles.



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Article II, Division 5. Commercial Zones

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Article VIII. Watercourse, Amenities, Safety and Habitat

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